

City of Eastvale I-15 Corridor Specific Plan

Prepared for
City of Eastvale



City of Eastvale

I-15 Corridor Specific Plan



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II. INTRODUCTION

Due to voter initiatives, the City of Eastvale incorporated on October 1, 2010 and the City of Jurupa Valley incorporated July 1, 2011. The *City of Eastvale I-15 Corridor Specific Plan*, as amended through January 2011 (“Specific Plan”) was part of a larger specific plan that encompassed portions of the City of Jurupa Valley. As part of that larger specific plan, public facility and infrastructure requirements are complete. The *City of Eastvale I-15 Corridor Specific Plan* has not been rewritten to reflect existing uses. Rather, it is modified to reflect Development Standards and Guidelines for the two remaining undeveloped planning areas located within the City of Eastvale.

A. PROJECT DESCRIPTION

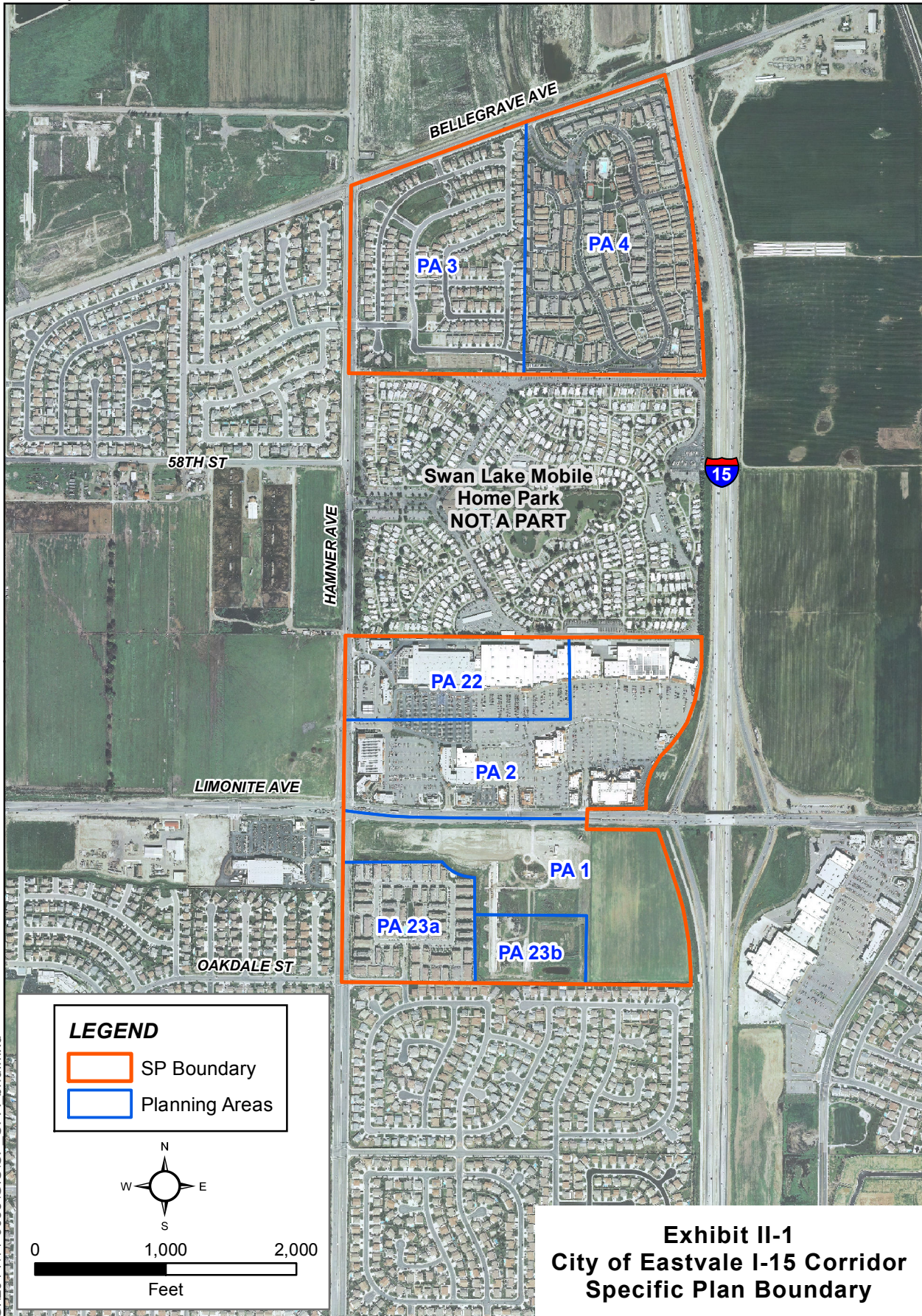
The *City of Eastvale I-15 Corridor Specific Plan* is bounded on the east by I-15, on the west by Hamner Avenue, on the north by Bellegrave Avenue, and to the south, approximately ¼ mile south of Limonite Avenue, as reflected in **Exhibit II-1, City of Eastvale I-15 Corridor Specific Plan Boundary**.

This Specific Plan is comprised of seven planning areas (Planning Areas 1, 2, 3, 4, 22, 23a, and 23b); all of which have been built out, with the exception of two. Planning Area’s 3 and 4 have been developed with single-family and multi-family residential dwelling units, one tot lot has been constructed as a part of the development of Planning Area 3, and Planning Areas 2 and 22 have been developed as Eastvale Gateways I (North) and II (South), a large commercial center accommodating several large commercial retailers as anchors. Planning Area A23a was constructed with 314 multi-family residential dwelling units and the northwest corner of Planning Area 1 is currently under construction with commercial-retail development. The Swan Lake Mobile Home Park located at 5800 Hamner Avenue, just north of Planning Area 22 and south of Planning Areas 3 and 4, is not a part of this Specific Plan.

Planning Area 23b and a portion of Planning Area 1 are the only planning areas within this Specific Plan that remain undeveloped. This Specific Plan provides for mixed land uses of commercial and high density residential. The guidelines set forth in this document ensure completion of Eastvale Gateway South by enforcing the continued development of this Specific Plan as planned, through implementation of its Development Standards and Guidelines.

This Specific Plan also changes the existing boundary of Planning Area 1 and Planning Area 23b, as reflected in **Exhibit II-2, Existing Planning Areas 1 and 23b Boundaries**, by moving the northern boundary line of Planning Area 23b 260 feet to the south, as shown in **Exhibit II-3, Proposed Planning Areas 1 and 23b Boundaries**, to provide for an additional 5 acres of commercial land use in Planning Area 1. The land use designation of this additional 5 acres in Planning Area 1 will change from High Density Residential (HDR) to Commercial-Retail (C-R). This provides for a total of 45 acres of commercial land use in Planning Area 1 to accommodate the proposed Plot Plan listed below in Section II.A.2. This change reduces the total residential land use acreage in Planning Area 23b to 10 acres, resulting in 5 fewer acres of residential land usage. However, to off-set this reduction, this Specific Plan changes the land use designation of Planning Area 23b from High Density Residential (H) to Highest Density

Residential (HHDR) under the current General Plan, with a maximum of 300 dwelling units or 30 dwelling units per acre (du/ac). The proposed change in land use would increase the number of dwelling units allowed in Planning Area 23b by 55.



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LEGEND

- SP Boundary
- Planning Areas

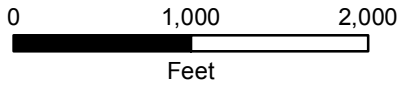
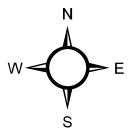
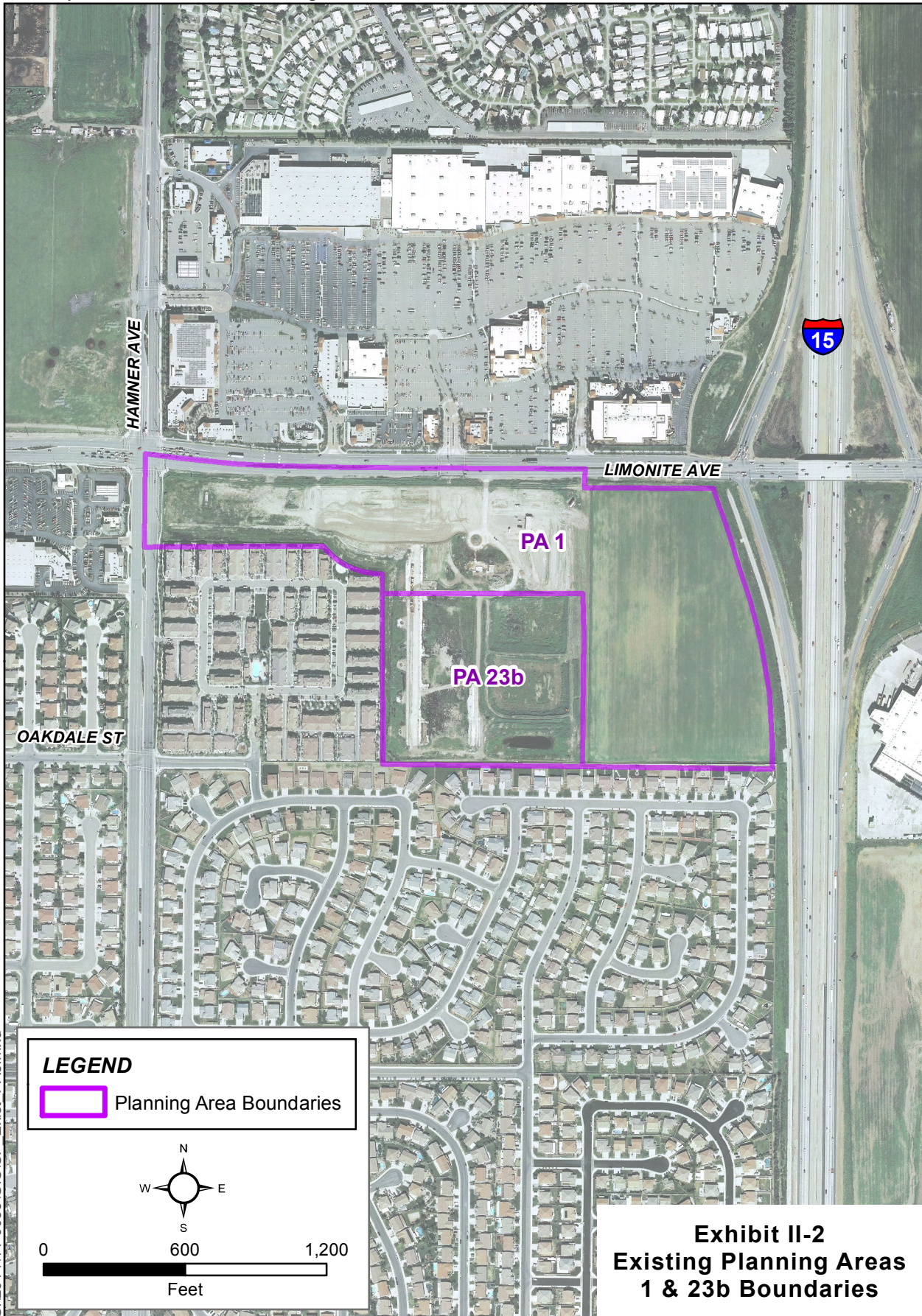


Exhibit II-1
City of Eastvale I-15 Corridor
Specific Plan Boundary

Sources: County of Riverside GIS, 2011;
 Egel Aerial, April 2010.



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Sources: County of Riverside GIS, 2011;
Egale Aerial, April 2010.

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Sources: County of Riverside GIS, 2011;
Egale Aerial, April 2010.

1. Location

The *City of Eastvale I-15 Corridor Specific Plan* is an assemblage of parcels totaling approximately 263.3 acres located in the City of Eastvale in western Riverside County. This Specific Plan is generally bounded by I-15 to the east, Hamner Avenue to the west, Bellegrave Avenue on the north, and approximately ¼-mile south of Limonite Avenue, as reflected in **Exhibit II-1**, above.

2. Proposed Land Use Applications

Rezoning of the Specific Plan is necessary to bring the zoning designations into conformance with the Land Use Development Plan adopted as a part of this Specific Plan. A Change of Zone application has been filed and is being processed concurrently with this Specific Plan. Planning Areas 23a and 23b are currently zoned Specific Plan (SP), so no change is necessary. The Change of Zone applies to Planning Areas 1, 2 and 22.

A General Plan Amendment is also necessary to bring the land use designations of undeveloped planning areas into conformance with the current General Plan. A General Plan Amendment has been filed and is being processed concurrently with this Specific Plan. The General Plan Amendment applies to Planning Area 23b.

General Plan Amendment: Proposes to change the land use designation of the southern 10 acres of Planning Area 23b from HDR (High Density Residential) to Highest Density Residential (HHDR) and the northern 5 acres of Planning Area 23b from HDR to Commercial-Retail (C-R).

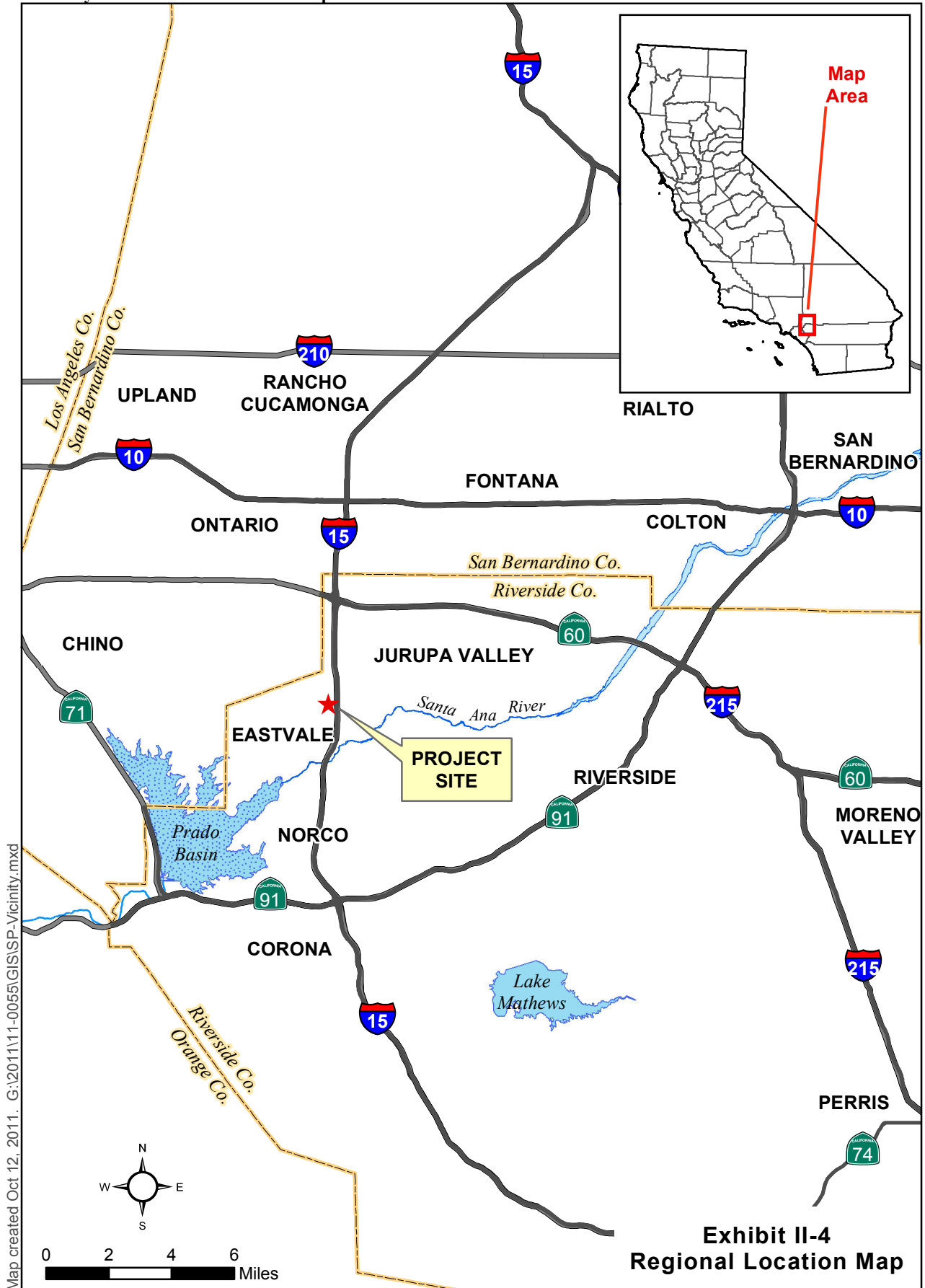
Change of Zone: Proposes to change existing zoning of Planning Areas 1, 2 and 22 from Scenic Highway Commercial (C-P-S) to Specific Plan (SP).

Plot Plan: Proposes development of an approximately 43,000 square foot 24 Hour Fitness health club and an approximately 11,340-square-foot commercial-retail building on approximately 7 acres located in Planning Area 1, established by the proposed change to planning area boundary between Planning Area 1 and Planning Area 23b.

Lot Line Adjustment: Proposes to move the existing southerly boundary of Parcel 7 of PM 35933 to accommodate the construction of a commercial-retail building currently proposed across the existing parcel line and to establish the boundary between the residential land use of Planning Area 23b and commercial land use of Planning Area 1.

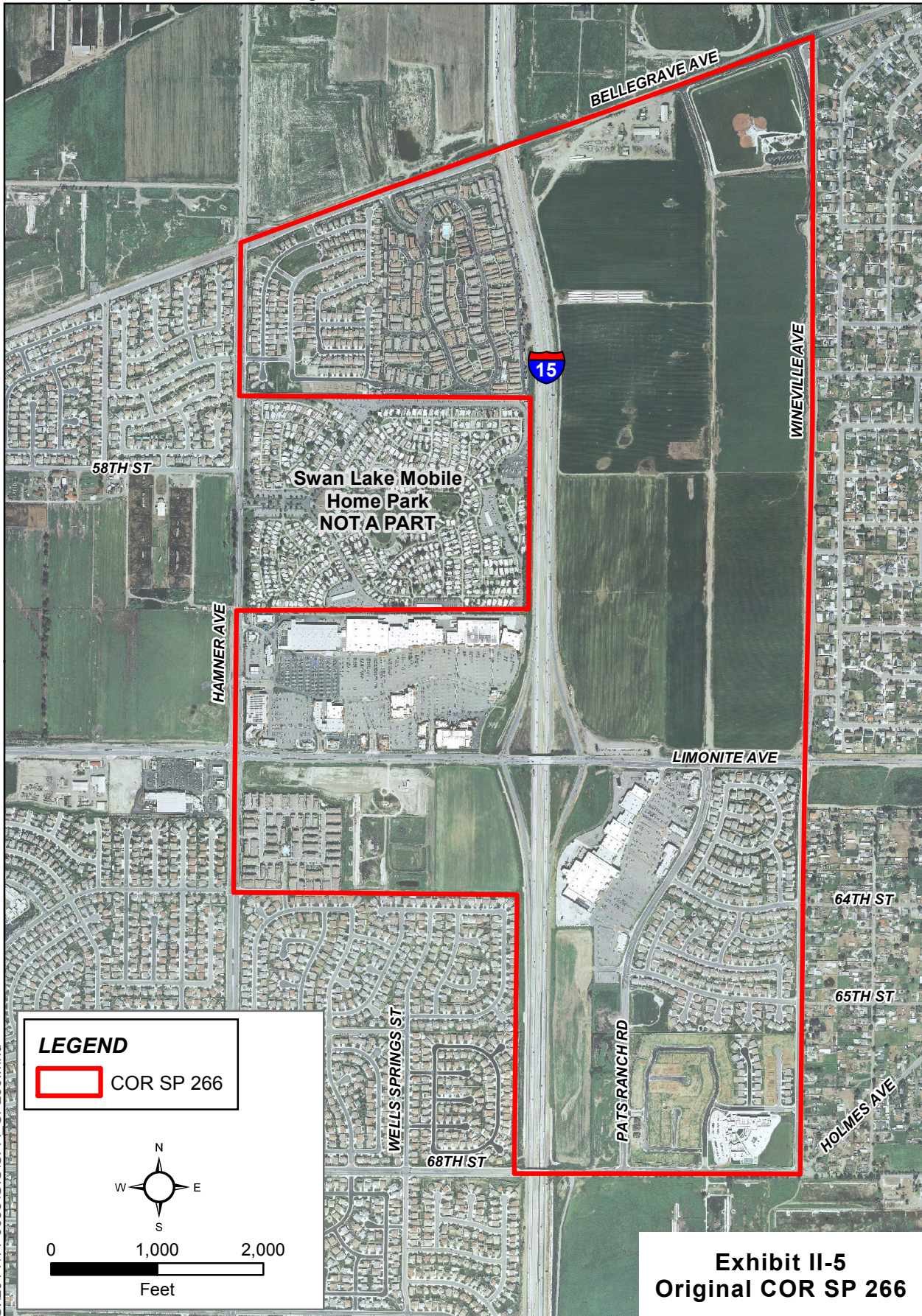
3. Background

On November 2, 1993, the County of Riverside Board of Supervisors adopted the original *County of Riverside I-15 Specific Plan No. 266 (COR SP266)*, which was an assemblage of parcels totaling approximately 701 acres located on the western edge of Riverside County within the boundaries of what was known as the Jurupa Community Plan, as reflected in **Exhibit II-4, Regional Location Map**. The



Map created Oct 12, 2011. G:\2011\11-0055\GIS\ISP-Vicinity.mxd

Exhibit II-4 Regional Location Map



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Sources: County of Riverside GIS, 2011;
Egel Aerial, April 2010.

ALBERT A. **WEBB** ASSOCIATES

purpose of *COR SP266* was to establish comprehensive guidance and regulations for development of the site which was, at the time, located in the communities of Jurupa and Mira Loma, both unincorporated areas of the County of Riverside. This was accomplished through land use designations and program development standards which served to provide adequate public facilities, protect significant environmental resources, and implement the goals and policies of the County of Riverside. As reflected in **Exhibit II-5, Original COR SP266 Boundary**, *COR SP266* was bounded on the east by Wineville Avenue, Hamner Avenue to the west, Bellegrave Avenue to the north, and 68th Street to the south on the east side of Interstate 15 (I-15), and just south of Limonite, on the west side of I-15. *COR SP266* was generally bisected by I-15 and situated between Bellegrave Avenue on the north and 68th Street to the south. Limonite Avenue was the major east-west circulation element. The Swan Lake Mobile Home Park located at 5800 Hamner Avenue, was not a part of *COR SP266*.

COR SP266 was prepared pursuant to the provisions of the California Government Code which authorizes local jurisdictions to prepare and adopt a Specific Plan for the development of any area included within a General Plan as a means of implementing, through detailed procedures and standards, the jurisdiction's General Plan. The intent of *COR SP266* was to initiate the development of a major new commercial core (known today as Eastvale Gateway), along with a balanced amount of residential development to serve both Riverside and San Bernardino counties. Commercial areas were situated along the axis of I-15 and Limonite Avenue. Commercial uses were compatible and complimentary to the freeway exposure afforded by I-15, as well as the community of neighborhoods surrounding *COR SP266*. *COR SP266* included several of those neighborhoods totaling 2,210 residential dwelling units. Maximum densities for residential development in individual planning areas ranged from 3.6 to 12.0 du/ac.

Concurrent with adoption of *COR SP266*, the County of Riverside Board of Supervisors adopted Change of Zone No. 5619 and General Plan Amendment No. 242. Change of Zone No. 5619 changed zoning on the properties located within the boundaries of *COR SP266* to zoning classifications consistent with *COR SP266* land use designations. It also changed the zoning from Heavy Agriculture – 10-acre minimum (A-2-10), to Specific Plan (SP). General Plan Amendment No. 242 changed the land use designations on these same properties from 1B (high), 2A (medium-high), 2B (medium), and C (Commercial) to Specific Plan (SP). At the time of *COR SP266*'s adoption, land use designations were consistent with the 1992 *Riverside County Comprehensive General Plan*, and was approved under a document known as the *Jurupa Community Plan*.

On December 4, 1989, the County of Riverside, in its role as Lead Agency for *COR SP266*, issued a Notice of Preparation (NOP) to the State Clearinghouse, responsible agencies, and other interested parties. The Environmental Impact Report (EIR) prepared for *COR SP266* (*EIR340*) was prepared pursuant to the County's authority to address potential impacts of *COR SP266* and was found to be consistent with the requirements of the California Environmental Quality Act (CEQA) and the County of Riverside *CEQA Implementation Guidelines*. *EIR340* was intended to provide comprehensive environmental documentation for all of the *COR SP266* project-related environmental issues, as identified by the Initial

Study and to serve as the Program EIR consistent with the provisions of Section 15168 of CEQA. *EIR340* provided County of Riverside staff and the general public with the information necessary to access the full extent of potential environmental impacts associated with the development of *COR SP266*. On November 2, 1993, the County of Riverside determination of *EIR340* was that potential *COR SP266* project and cumulative impacts to air quality and agriculture, could not be fully mitigated and therefore, adopted a Statement of Overriding Considerations stating that *COR SP266* benefits outweighed and rendered acceptable, the unavoidable adverse environmental impacts created by *COR SP266*. *EIR340* now serves as the base environmental document for any future development proposals within the *COR SP266* area.

Subsequent to the 1993 adoption of *COR SP266*, the County of Riverside amended *COR SP266* several times to accommodate various land use proposals and changes. On December 23, 2002, the County of Riverside Board of Supervisors approved the *COR SP266, Amendment No. 1 (COR SP266A1)* and Change of Zone No. 6705, which included changes to land uses affecting Planning Areas 22 and 23. *COR SP266A1* changed the land use designation of Planning Area 22 from Commercial Office (C-O) to Commercial (C). It also allowed for a senior citizen housing alternative to be developed on a portion of Planning Area 23. This allowed for the development of an additional 245 senior housing dwelling units with a maximum density of 36 du/ac with the remaining portion of Planning Area 23 to be developed with a maximum of 322 multi-family residential dwelling units with a maximum density of 20 du/ac. *EIR340, Addendum No. 1 (EIR340A1)* was prepared to analyze impacts proposed by *COR SP266A1*. Based on *EIR340A1*, the County of Riverside determined that *COR SP266A1* and all related land use applications would not result in potentially significant environmental effects beyond those originally analyzed in *EIR340* or could mitigate new impacts to a level of less than significant and therefore, approved *EIR340A1* on December 23, 2002.

On March 11, 2008, the County of Riverside Board of Supervisors approved the *COR SP266, Amendment No. 2/Substantial Conformance No. 4 (COR SP266A2/SC4)* along with Change of Zone No. 7480. *COR SP266A2/SC4* transferred 7.9 acres from Planning Area 1 to 23, thereby reducing the acreage in Planning Area 1 from 47.9 to 40.0 acres and increasing acreage in Planning Area 23 from 26.8 to 34.7 acres. In addition, Planning Area 23 was divided into two planning areas: 23a consisting of 19.7 acres and 23b consisting of 15 acres. Zoning was changed on a portion of Planning Area 1 that was zoned General Residential (R-3) to Scenic Highway Commercial (C-P-S) and a portion of Planning Area 23 that was zoned Scenic Highway Commercial (C-P-S) was changed to General Residential (R-3). Environmental Assessment No. 41241 was prepared to analyze impacts proposed by *COR SP266A2/SC4*. Based on the Environmental Assessment, the County of Riverside determined that *COR SP266A2/SC4* and all related land use applications would not necessitate changes in or additions to *EIR340*. Accordingly, a “Nothing Further Required” Environmental Assessment was prepared.

On March 3, 2009, *COR SP266, Substantial Conformance 3 (COR SP266SC3)* was approved, which reduced the total number of dwelling units (DU) in Planning Area 23 from 322DU to 314DU. A Mitigated Negative Declaration for Environmental Assessment No. 41089 was prepared for *COR SP266SC3* and

adopted March 3, 2009 by the County of Riverside based on the findings incorporated into the Environmental Assessment and the conclusion that the Project would not have a significant effect on the environment.

Development Today and Modifications to this Document

Due to voter initiatives, *COR SP266* is no longer located within the unincorporated portion of the County of Riverside as the City of Eastvale incorporated on October 1, 2010 and the City of Jurupa Valley incorporated July 1, 2011. The boundary line between the two new cities is I-15. Thus, a portion of *COR SP266* is located within each city. As such, Planning Areas 1, 2, 3, 4, 22, 23a, and 23b are the only planning areas located within the *City of Eastvale I-15 Specific Plan* (“Specific Plan”) boundary. All other planning areas that lie east of I-15 in the newly incorporated City of Jurupa Valley and their associated Development Standards and Guidelines, have been removed from this Specific Plan, as well as all references to the County of Riverside, except where applicable. The numbering of Planning Areas located within the City of Eastvale have not been modified and remain unchanged in this Specific Plan.

All planning areas within the boundaries of this Specific Plan have been developed with the exception of Planning Area 23b and a portion of Planning Area 1. Since the inception of *COR SP266*, all of the planning area densities and development have been constructed in accordance with the original *COR SP266*.

COR SP266 was originally approved under the previous *1992 Riverside County Comprehensive General Plan* document and a document known as the *Jurupa Community Plan*. Thus, *COR SP266* and its subsequent amendments reflect land use designations and policies consistent with the *1992 Riverside County Comprehensive General Plan* and *Jurupa Community Plan* documents. In 2003, the County of Riverside adopted a new General Plan, which the City of Eastvale adopted upon its incorporation. The *City of Eastvale Specific Plan* is built out with the exception of Planning Area 23b and a portion of Planning Area 1. Planning Areas 23b and 1 are the only planning areas to propose a land use change. Additionally, the *City of Eastvale I-15 Corridor Specific Plan* is no longer associated with the *Jurupa Community Plan*. This Specific Plan now lies within the Eastvale Area Plan.

The original *COR SP266* site was traversed by a utility alignment for Southern California Edison (SCE). This 245 foot wide alignment was owned in fee title by SCE. The only existing line within it was a 115 KV line located south of Limonite Avenue. This alignment was consolidated in *COR SP266* and reflected as a Utility Corridor. Negotiations with SCE were required to gain access to the alignment area, such as for commercial parking. Another SCE transmission line crossed *COR SP266*, running from the north-south SCE property alignment, easterly to Wineville Avenue. This line was parallel to, and approximately 720 feet north of Limonite Avenue, and located within property owned by SCE. No utilities were ever installed in the area identified in *COR SP266* as the Utility Corridor and SCE did not have any plans for utilizing this corridor. Consequently, the lands were sold to private owners. As such, all Utility Corridor references have been removed from this document.

As the *City of Eastvale I-15 Corridor Specific Plan* is part of the larger *COR SP266*, and all planning areas with the exception of two have been developed, a majority of its public facility and infrastructure requirements have been completed. Park requirements have been accomplished through the larger *COR SP266*. As such, the Phasing Plan is no longer applicable and all references to phasing have been removed from this document. Only undeveloped planning areas (23b and a portion of 1) shall continue to enforce the guidelines of this Specific Plan and develop accordingly.

Text, tables, and exhibits found in Section IV include changes to land use, density, acreage, and boundaries only as discussed above.

B. GOALS AND OBJECTIVES

The following Goals and Objectives helped to shape the content of the *City of Eastvale I-15 Corridor Specific Plan* and its land use designations. As such, this Specific Plan will continue to meet the following:

1. The Specific Plan is intended to take advantage of the premier location by creating a new primary node of commercial and business opportunities.
2. The Specific Plan will provide substantial new employment opportunities, far exceeding the normal expectation for a project jobs-to-housing ratio, in a convenient and centralized location thereby helping to offset the jobs/housing imbalance.
3. The Specific Plan is situated adjacent to over 4,000 acres of industrially designated land that will require convenient housing opportunities for both management and employees.
4. The Specific Plan will provide for residential dwelling units, the majority being for sale units with some rental units, creating housing opportunities convenient to major employment bases for a variety of income levels. Initial marketing strategies will be geared toward the primary homebuyer.
5. Commercial marketing will be initially directed toward providing neighborhood and community level goods and services with the long-term intent of establishing a super-regional core will service and expanded geographic market.

C. OVERALL DESIGN OBJECTIVES

The overall design objective to introduce a more urban type of development to the region while maintaining sensitivity to adjacent residential neighborhood uses has been accomplished through the development of all planning areas located within the City of Eastvale, with the exception of Planning Area 23b and a portion of Planning Area 1, as well as the success of the Eastvale Gateways I (North) and II (South) Commercial Center. This Specific Plan's structure is based on a development concept driven by community integration and compatibility. Specific policies used to achieve this are listed in the individual Planning Area Development Standards in this document. This overall concept was formulated by focusing on the following comprehensive goals:

1. To establish a community and regional style that emphasizes continuity and compatibility in a setting with convenient access to commercial, recreational, regional transportation elements, and open space opportunities and amenities.
2. To establish a functionality efficient street network which provides for superior internal circulation, appropriate integration with the existing and proposed elements of the external street network and limited traffic impact on the existing area.
3. To provide all necessary infrastructural utilities through an underground transmission network designed and located to be easily accessed, maintained and repaired.
4. To provide a coordinated land use plan which emphasizes:
 - a. A logically organized land use pattern to ensure the proper relationship and interface between different uses; and
 - b. The proper location, orientation, and proportion of residential and commercial uses to promote appropriate circulation distributions.
5. To foster community identification through the development of a uniform design concept, including architectural and landscape guidelines, which will affect aesthetically coordinated project components.
6. To create a contemporary design theme that will compliment and improve upon the existing regional character.

These Specific Plan design goals have been expanded and refined throughout this Specific Plan document as detailed in the Development Standards and Guidelines located in Section IV.

D. AUTHORITY AND SCOPE/PLAN PROCESSING

1. Purpose and Authority of the Specific Plan

The purpose of the *City of Eastvale I-15 Corridor Specific Plan* is to enforce the continued development of the remaining undeveloped Planning Areas within this Specific Plan to ensure development is completed as outlined in the Development Standards and Guidelines of this Specific Plan. The City of Eastvale shall be authorized to adopt and implement the changes set forth in this Specific Plan.

A Specific Plan is a regulatory document which serves as zoning for a subject property. As such, this Specific Plan must be consistent with the intent and purpose of the *City of Eastvale General Plan*. Subsequently, all future development proposals, plans, maps, and agreements must be consistent with the *City of Eastvale I-15 Corridor Specific Plan*. Projects which are found to be consistent with this Specific Plan will be deemed consistent with the intent of the Eastvale Area Plan. This document has been prepared in compliance with the City of Eastvale Form and Content requirements for Specific Plans.

The *City of Eastvale I-15 Corridor Specific Plan* is being processed concurrently with the following land use applications:

- Zone Change Case No. 10-0084: Proposes to change the existing zoning designations of Planning Areas 1, 2 and 22 from Scenic Highway Commercial (C-P-S) to Specific Plan (SP).
- General Plan Amendment Case No. 10-0084: Proposes to change the land use designation of southern 10 acres of Planning Area 23b from HDR (High Density Residential) to Highest Density Residential (HHDR) and the northern 5 acres of Planning Area 23b from HDR to Commercial-Retail (C-R).
- Plot Plan: Proposes development of an approximately 43,000-square-foot 24-Hour Fitness and an approximately 11,340-square-foot commercial-retail building (Shops 2) on approximately 7 acres located in Planning Area 1, established by the proposed change to the planning area boundary between Planning Areas 1 and 23b.
- Lot Line Adjustment: Proposes to move the existing southerly boundary of Parcel 7 of PM 35933 to accommodate the construction of a commercial-retail building (Shops 2) currently proposed across the existing parcel line and to establish the boundary between the residential land use of Planning Area 23b and commercial land use of Planning Area 1.

2. Purpose and Authority of the EIR Addendum

Environmental Impact Report No. 340, Addendum No. 2 (EIR340A2) prepared for the *City of Eastvale I-15 Corridor Specific Plan* was prepared pursuant to the City's authority to address potential impacts of this Specific Plan and with the requirements of the California Environmental Quality Act (CEQA) as set forth in Public Resources Code Section 21000, et seq., and the *State CEQA Guidelines*, Title 14 of the California Code of Regulations Section 15000, et seq. *EIR340A2* is intended to provide comprehensive environmental documentation for all of the Project-related environmental issues, as identified by the Initial Study and to serve as the Addendum consistent with the provisions of Section 15164 of the *State CEQA Guidelines*.

1) Scope of the EIR Addendum

As noted above, the EIR Addendum is an informational document used in local and state agency decision-making processes. It is not the purpose of the EIR Addendum to either recommend approval or denial of a project or to present political, social, or economic reasons to project approval or denial. The EIR Addendum identifies potential environmental issues associated with this Specific Plan beyond those analyzed in EIR340.

2) Intended Uses of the EIR Addendum

The accompanying EIR Addendum prepared for the *City of Eastvale I-15 Corridor Specific Plan* has been prepared pursuant to the City's authority and addresses potential impacts of this Specific Plan and is consistent with the requirements of the CEQA. This EIR Addendum is intended to provide comprehensive environmental documentation for all of the project-related environmental issues, as identified by the Initial Study. Applications covered by the Addendum, are as follows:

- City of Eastvale I-15 Corridor Specific Plan (Case No. 10-0084)
- General Plan Amendment (Case No. 10-0084)
- Change of Zone (Case No. 10-0084)
- Development Applications: Plot Plan (Case No. 10-0084) and Lot Line Adjustment (Case No. 10-0084)

III. SPECIFIC PLAN ZONING ORDINANCE

ALL DEVELOPMENT WITHIN THE SPECIFIC PLAN WILL BE DEVELOPED ACCORDING TO THE CORRESPONDING ZONING STANDARDS CONTAINED IN THE CITY OF EASTVALE ZONING CODE.

IV. SPECIFIC PLAN

A. PROJECT-WIDE DEVELOPMENT PLANS AND STANDARDS

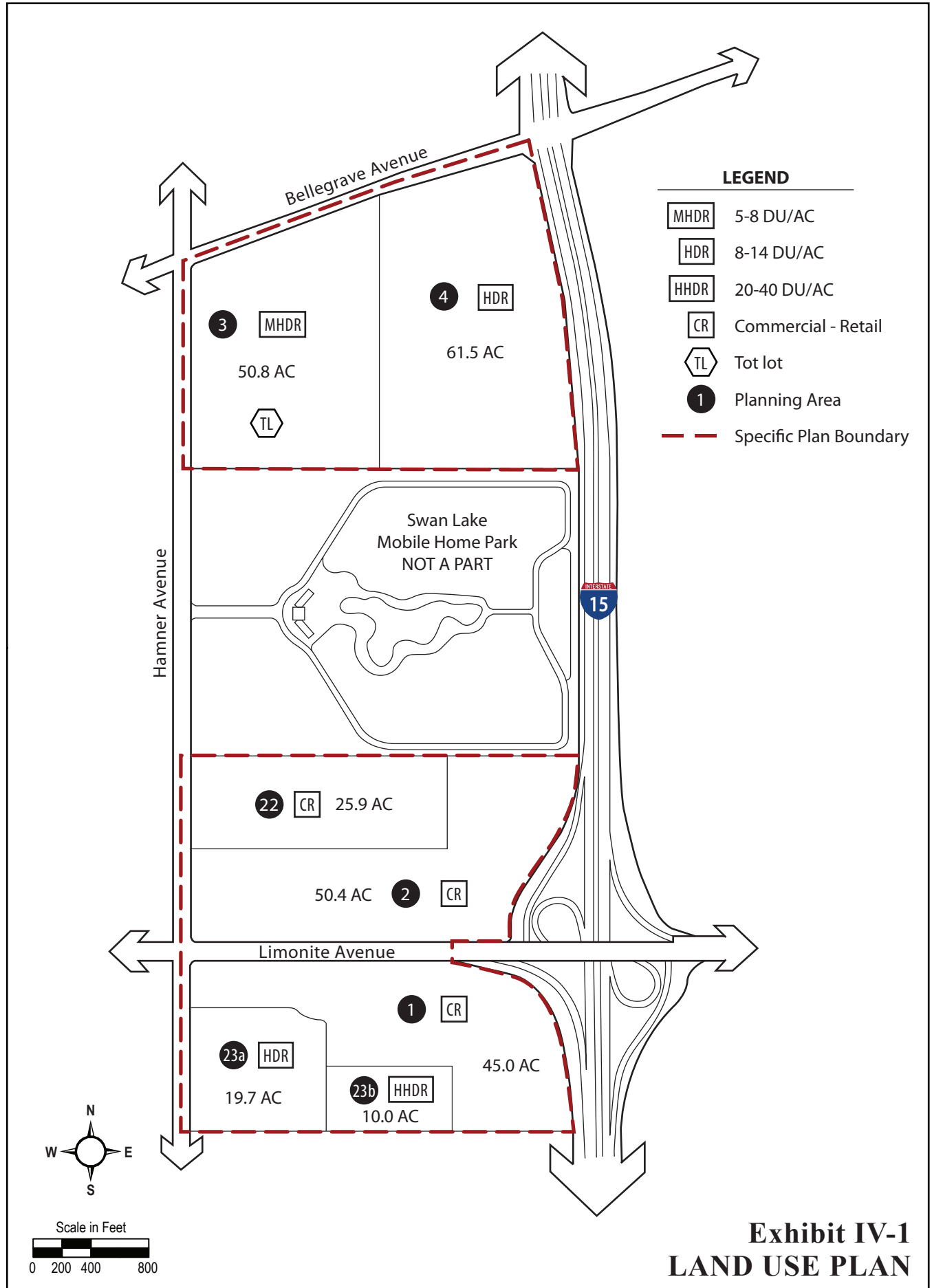
1. Comprehensive Land Use Plan

The *City of Eastvale I-15 Corridor Specific Plan* (“Specific Plan”) is a planned community of 263.3 acres, which includes a mix of commercial and residential uses. All but two Planning Areas within this Specific Plan have been developed and a majority of public facilities and infrastructure has been constructed. A total of 121.3 acres of commercial development is intended to service the full range of local, community, and regional needs. This Specific Plan also provides for a total of 1,596 residential dwelling units on 142 acres. Average residential density for this Specific Plan is 4.8 du/ac for single-family residences and 19.3 du/ac for multi-family residences. **Table IV-1, Land Use Summary**, shows the specific land use breakdown by planning area, acreage, and density, and reflects the land use configurations illustrated in **Exhibit IV-1, Land Use Plan**.

The Development Standards and Guidelines provided in this section have been developed for the *City of Eastvale I-15 Corridor Specific Plan* and have been tailored to be sensitive to the physical characteristics of the site and its surroundings. The *City of Eastvale I-15 Corridor Specific Plan* Development Standards and Guidelines will be used to complete the remainder of development within this Specific Plan. These Development Standards and Guidelines will enforce the continued development of undeveloped Planning Areas to ensure that consistently high quality development continues to occur within this Specific Plan, thus protecting and enhancing the investment of all those uses located within this Specific Plan area. These Development Standards and Guidelines provide a documented basis for directing and evaluating the planning and design of improvements of the remaining undeveloped Planning Areas (1 and 23b) within this Specific Plan, and provide procedures upon which the City’s development review process can be based.

Table IV-1, Land Use Summary

PLANNING AREA	DESIGNATION	ACRES (GROSS)	UNITS	D/U PER ACRE
3	MHDR	50.8	244	4.8
Total Single-Family		50.8	244	4.8
4	HDR	61.5	738	12.0
23a	HDR	19.7	314	15.9
23b	HHDR	10.0	300	30.0
Total Multi- Family		91.2	1,352	19.3
1	CR	45.0	--	--
2	CR	50.4	--	--
22	CR	25.9	--	--
Total Commercial		121.3	--	--
TOTAL		263.3	1,596	



Land Use Standards

- a. The *City of Eastvale I-15 Corridor Specific Plan* shall be developed with a maximum of 1,596 residential dwelling units on 142 acres as shown on **Table IV-1**, above.
- b. The total number of dwelling units for each Planning Area, as reflected in Table IV-1, shall be determined through the appropriate tract and/or site plan application up to the maximum units identified. This shall be based upon, but not limited to, the following factors:
 - 1) Availability of utility services.
 - 2) Adequate access and circulation facilities.
 - 3) Sensitivity to landforms and other environmental constraints.
 - 4) Sensitivity to neighborhood design through appropriate lot and street layouts.
 - 5) Any policies, programs and goals which are established in the General Plan.
- c. Units shall be consistent with those set forth in Table IV-1 above, with total unit count not to exceed that indicated for any given planning area, except as provided for in Section IV.D, Implementation Programs.
- d. The development of this Specific Plan shall comply with the applicable provisions of the City of Eastvale Building Code, and the applicable provisions of all City agencies.
- e. Water and sewage disposal facilities shall be installed in accordance with the requirements and specifications of the City of Eastvale Public Works Department and the Jurupa Community Services District.

Commercial Component

A commercial component is included in the *City of Eastvale I-15 Corridor Specific Plan* in response to two primary factors. The two factors are anticipated long-range market demand and the convenient freeway access and exposure. The commercial areas total approximately 121.3 acres and are intended to provide community, and ultimately, regional goods and services. The commercial areas are controlled by various commercial sections in the City Zoning Code.

Commercial Standards

- a. The commercial uses shall be developed so as to be consistent with this Specific Plan requirements and standards.
- b. The following general standards shall apply to the commercial development:
 - 1) All development shall comply with the development standards of the City Zoning Code.
 - 2) Parking shall be provided as required by the City Zoning Code.
 - 3) Building elevations shall include full roof treatments and all mechanical roof-mounted equipment shall be screened from view from public highways.
 - 4) Wherever commercial development abuts residential development, either at the property line or across a street or road, the commercial development shall be designed to, and incorporate design features which, minimize negative impacts such as glare, light, odors, and noise.
 - 5) Storage areas, loading areas, and trash receptacles shall be located and screened so as not to impose adverse health and noise impacts upon adjoining areas.
 - 6) Signage will be consistent with standards set forth by City of Eastvale Ordinance and the Specific Plan Design Guidelines.
 - 7) All implementing project lighting shall be in accordance with the City of Eastvale Lighting Ordinance.
 - 8) All commercial areas shall be maintained by the property owner or Merchant's Association, as appropriate.

Residential Component

The residential component of the *City of Eastvale I-15 Corridor Specific Plan* is designed to satisfy perceived demand in the area for the residential product types. This plan has also taken the physical characteristics of the site into consideration. Anticipated housing demand in the area is for primary homes in a single-family detached configuration, as well as rental units in a multi-family setting. The primary home market is in close proximity to shopping, schools, and recreation areas.

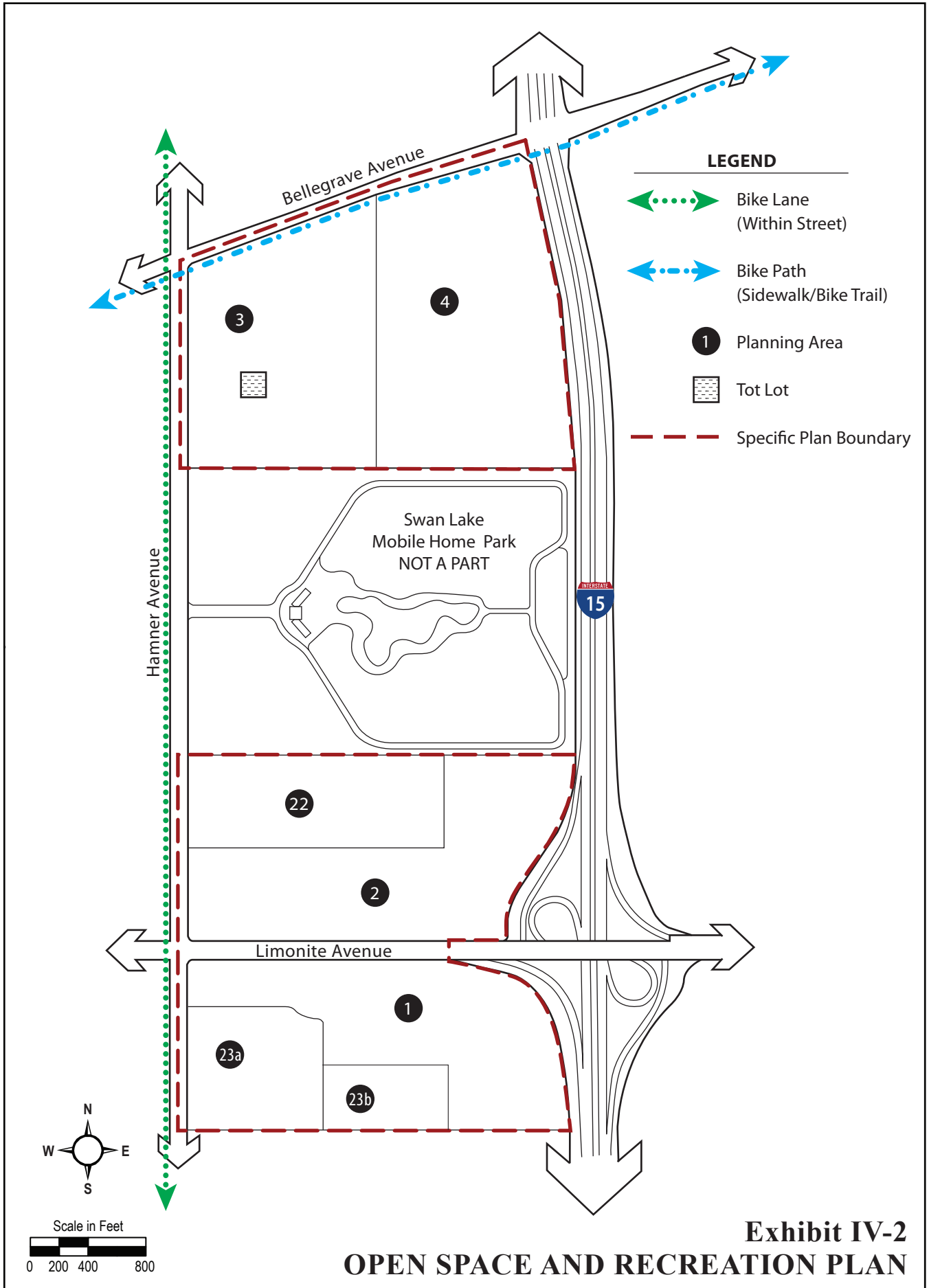
Controls on residential development levels for the *City of Eastvale I-15 Corridor Specific Plan* are as follows:

- 1) The maximum number of dwelling units indicated in the land use summary (Table IV-1, Land Use Summary) may not be exceeded.
- 2) The maximum number of dwelling units allocated to each phase of this Specific Plan may not be exceeded, except as provided for in Section IV.D, Implementation Programs.
- 3) Implementing project densities may not exceed the density range specified for planning areas, except as provided for in Section IV.D, Implementation Programs.

Recreation and Open Space Component

The Recreation and Open Space Plan, shown on **Exhibit IV-2, Open Space and Recreation Plan**, illustrates the recreational components of the plan. This Specific Plan provides for one tot lot that was constructed as a part of the development for Planning Area 3. All park requirements were accomplished in the original COR SP266.

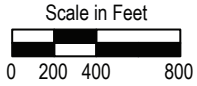
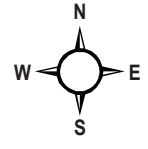
A network of bicycle lanes is included within this Specific Plan. The bike lane along the west side of Hamner Avenue is identified in the General Plan as a Class II lane, which is a delineated lane within the street pavement. This will be the requirement of the adjacent property developer. This Specific Plan includes a trail that is a combination pedestrian/bike path, located on the south side of Bellegrave Avenue. The path is similar to a Class I bike path in that the paved trail is separated from the street pavement.



LEGEND

- Bike Lane (Within Street)
- Bike Path (Sidewalk/Bike Trail)
- Planning Area
- Tot Lot
- Specific Plan Boundary

Swan Lake
Mobile Home Park
NOT A PART



**Exhibit IV-2
OPEN SPACE AND RECREATION PLAN**

2. Circulation Plan

Circulation Improvements to general plan designated roadways where roadways front existing development, have been constructed as described below. All existing uses comply with the Circulation Development Standards and Guidelines set forth below. Only undeveloped Planning Areas (1 and 23b) shall be required to continue the enforcement of these Circulation Development Standards and Guidelines and develop accordingly, as indicated below.

Highway Designations

Primary access to this Specific Plan is provided via Interstate-15 and Limonite Avenue. Interstate-15 is a six-lane freeway with access ramps at Limonite Avenue. Secondary access is available from Hamner Avenue and Bellegrave Avenue. **Table IV-2, Existing Circulation Improvements**, lists the improvements made to roadways.

All roadways and street improvements shall be designed and constructed in accordance with standards established by the City of Eastvale Public Works Department. The Circulation Plan is depicted in **Exhibit IV-3, Circulation Plan**, and the access plan is depicted in **Exhibit IV-4, Access and Control Plan**; and typical sections are shown in **Exhibit IV-5 through Exhibit IV-7, Circulation Element Sections**.

Table IV-2, Existing Circulation Improvements

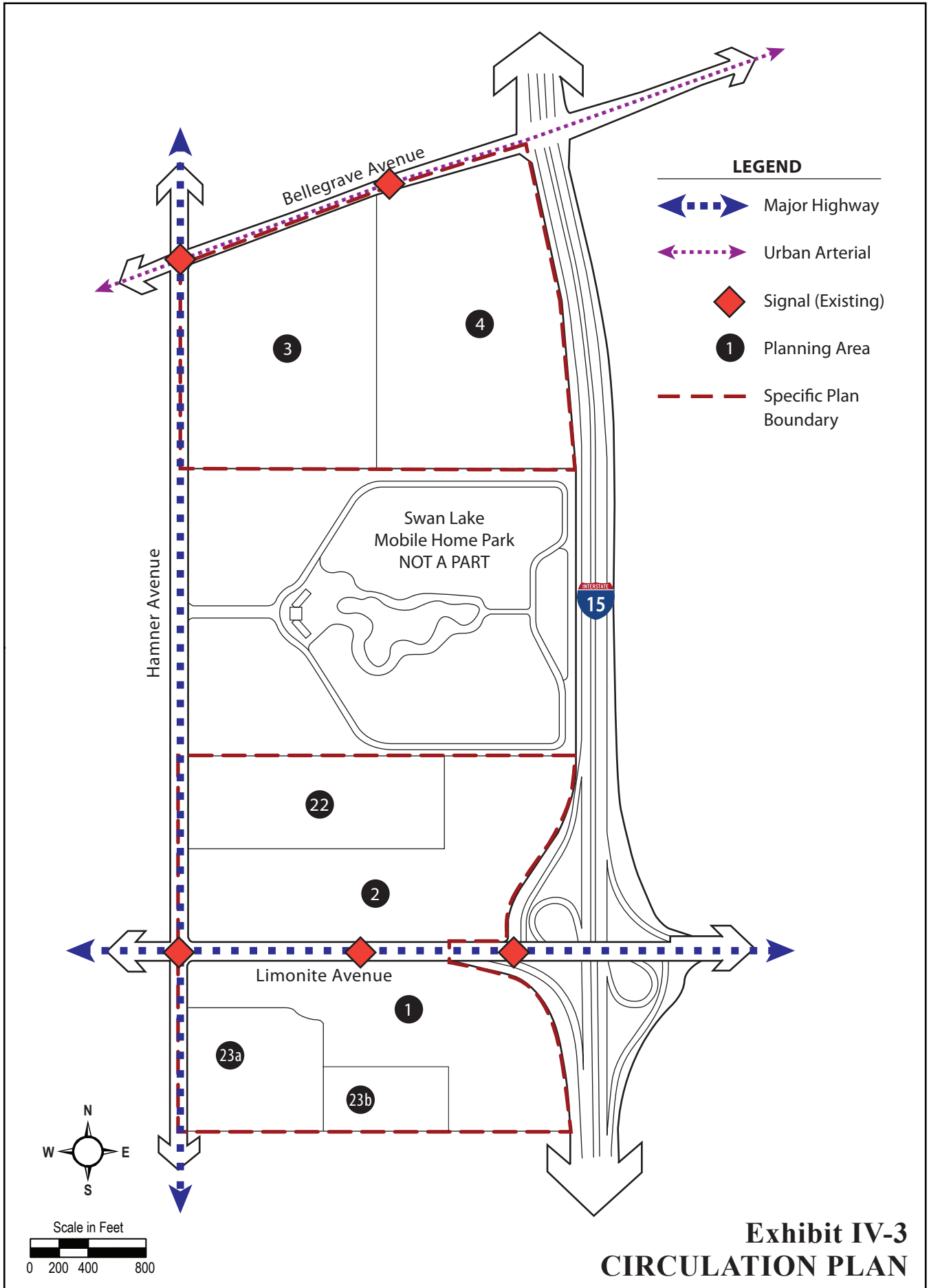
Street	Segment	General Plan Designation ¹	Proposed Designation	R.O.W.
Limonite	Hamner to I-15	M	UA	152'
Hamner	Bellegrave to S.W.C. Planning Area 23a	SP	UA	152'
Bellegrave	I-15 to Hamner	M	M	118'

¹ Source: City of Eastvale General Plan, Circulation Study Area No. 1

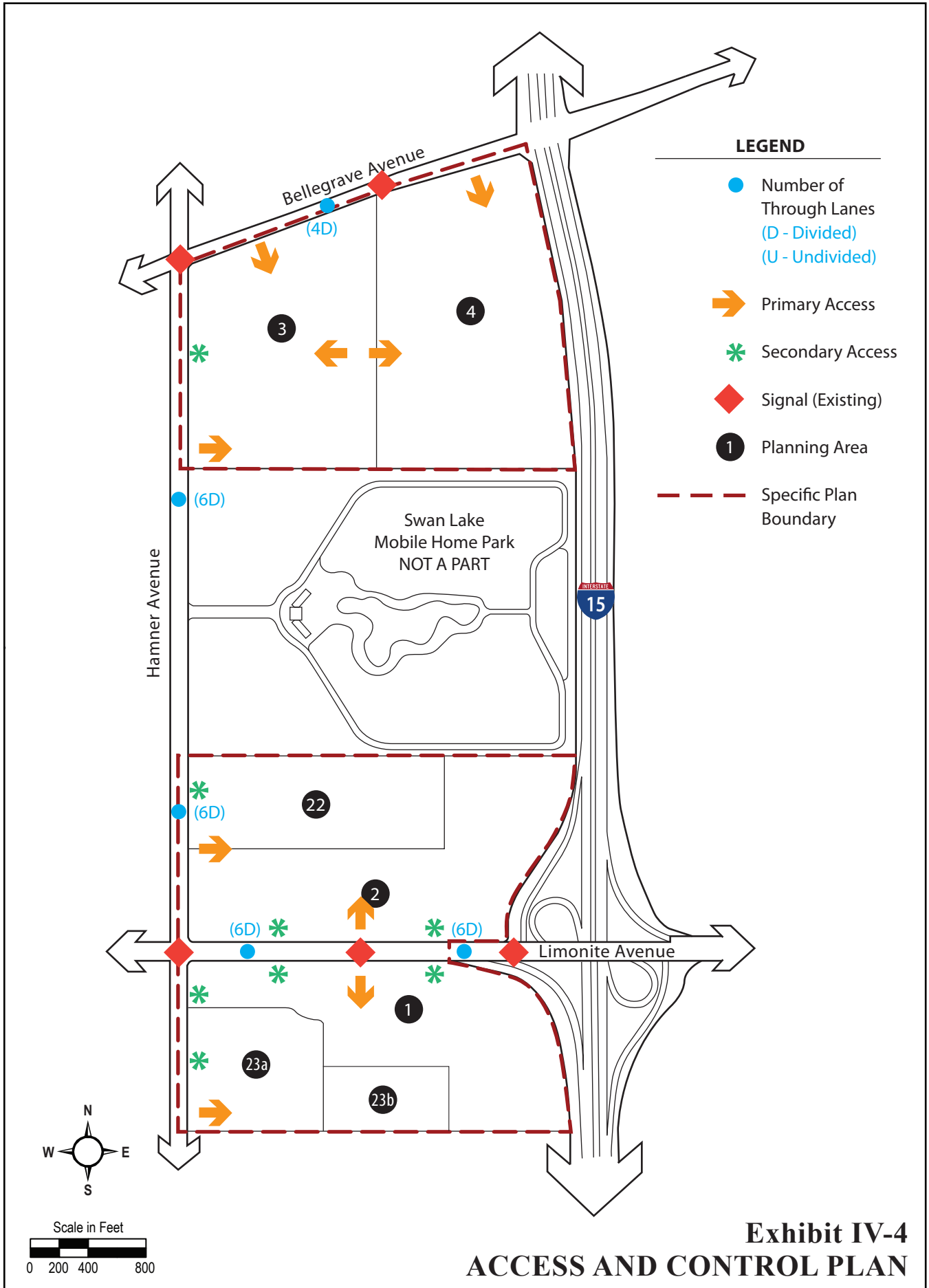
UA – Urban Arterial

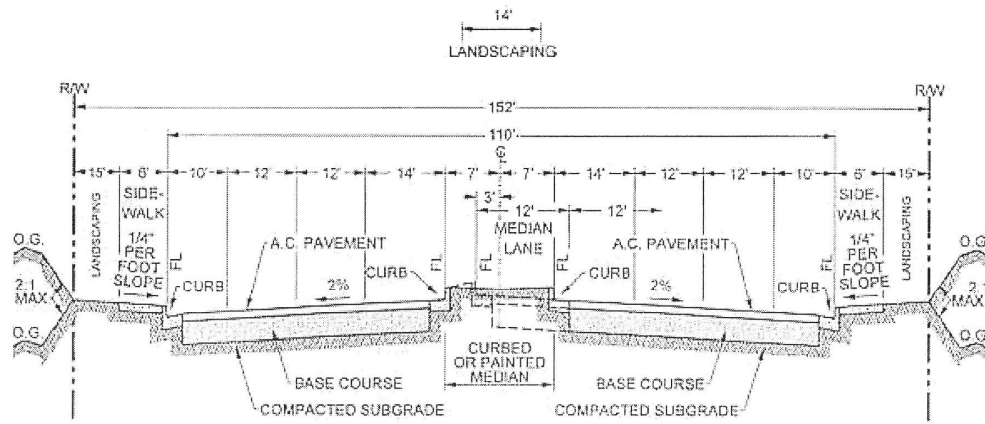
SP – Specific Plan

M – Major

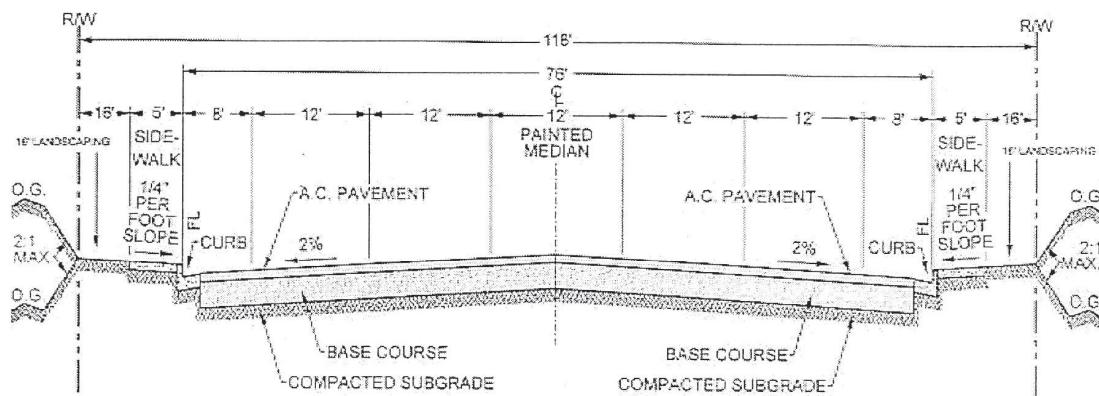


**Exhibit IV-3
CIRCULATION PLAN**

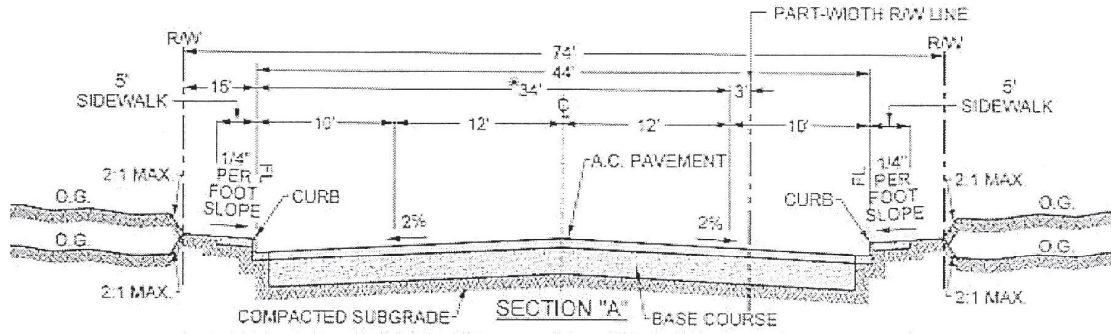




URBAN ARTERIAL
LIMONITE AVENUE

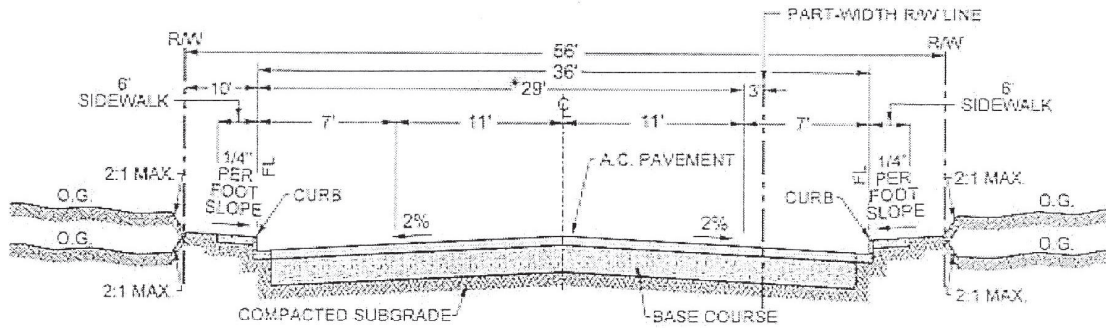


MAJOR HIGHWAY



*PART-WIDTH STREET SECTION FOR ALL COLLECTOR STREETS: 34' IMPROVEMENTS ON 52' R/W

COLLECTOR STREET



* PART-WIDTH STREET SECTION: 29' IMPROVEMENTS ON 42' R/W.

GENERAL LOCAL STREET

Development Standards

- a. Any application for any subdivision within the specific plan boundary (including a Schedule I Parcel Map) shall cause the design of the specific plan master planned infrastructure within the final map boundaries; exceptions are the division of land that has no parcel less than 40 acres or that is not less than a quarter of a quarter section. Specific Plan Schedule I Parcel Maps shall design the street system shown therein.
- b. All implementing projects, including subdivisions within the specific plan boundary, shall be subject to the Development Monitoring Program.
- c. All roadways intersecting four-lane facilities or greater shall be a minimum of 66 feet of right-of-way and constructed in accordance with standards set forth by City of Eastvale Ordinance, from the four-lane facility to the nearest intersection.
- d. All typical sections shall be per standards set forth by City of Eastvale Ordinance, or as approved by the City of Eastvale Public Works Department.
- e. All intersection spacing and/or access openings shall be per standards set forth by City of Eastvale Ordinance, or as approved by the City of Eastvale Public Works Department.
- f. Landscaping along arterial, major, and secondary highways shall be consistent with the applicable Landscape Design Guidelines in Section IV.C.3.
- g. Any landscaping within public road rights-of-way requires approval by the City of Eastvale Public Works Department and assurance of continuing maintenance through the establishment of a landscape maintenance district or similar mechanism as approved by the City of Eastvale Public Works Department.
- h. Non-vehicular circulation modes accommodated shall be consistent with **Exhibit IV-2, Open Space and Recreation Plan**. This Specific Plan is not responsible for the bike trail on the west side of Hamner Avenue.
- i. All bike trails developed as part of this Specific Plan are approved by the City of Eastvale Public Works Department.
- j. Drainage – This Specific Plan proposes no facilities to be maintained by the City of Eastvale Public Works Department. Therefore, all facilities other than facilities to be constructed in the road rights-of-way will be either private or be Flood Control District facilities.
- k. Sidewalks shall be required on both sides of streets. Unless otherwise indicated on **Exhibit IV-10, Parkway Plan**, below, all sidewalks shall be located per standards set forth by City of Eastvale Ordinance. This Specific Plan is not responsible for sidewalks on the north side of Bellegrave Avenue or the west side of Hamner Avenue.

Circulation Guidelines

- a. Design standards follow development criteria set forth in the Circulation Element of the General Plan.
- b. Commercial areas should be designed in a manner that facilitates future public transportation options. Appropriate access for bus stopping and turning movements should be incorporated as deemed necessary by the Riverside Transit Authority.
- c. Commercial – Per the General Plan, "Neighborhood Commercial Uses must be located along Secondary or greater highways, at or near intersections with Secondary Highways."
- d. Schools/Parks – The City of Eastvale Public Works Department policy regarding streets adjacent to school sites and park sites requires a minimum of 66' right-of-way (Standard 103).
- e. The circulation system has considered and coordinated roadway alignments with developments adjacent to this Specific Plan area.
- f. Bicycle paths and pedestrian trails have been integrated with the street system and adjacent developments where possible.
- g. Circulation design should provide for a safe and adequate means of ingress and egress of vehicular and pedestrian traffic to and within this Specific Plan.
- h. Mid-block crosswalks are not allowed.
- i. Circulation design should provide for access of emergency vehicles necessary to serve this Specific Plan area.
- j. Circulation design should provide for the most economical construction and maintenance of the necessary streets within this Specific Plan consistent within the circulation objectives of this Specific Plan.
- k. All public streets provide a level of street lighting designed to protect the health, safety and welfare of those residing or employed in, and passing through the Specific Plan area.
- l. Driveways and drives should be designed to a grade and alignment that will provide the maximum of safety and convenience for vehicular, emergency and pedestrian use in a manner which will not interfere with drainage or public use of the sidewalks and/or streets.
- m. Driveways/Access Points –All driveways/access points shall conform to City of Eastvale Public Works Department standard access spacing, depending upon the streets' classifications.
- n. The Limonite Avenue freeway overcrossing and freeway interchange shall be widened through a road and bridge benefit district or other regional funding mechanism.

Street Standards

- a. Relationship to Established Standards – Specific Plan street standards, as provided in **Exhibit IV-3, Circulation Plan**, are in conformance with the present design standards.
- b. Geometrics – The design of all streets incorporate horizontal and vertical curves adequate to provide safe vehicular travel.
- c. Each subdivision shall comply with the on-site and off-site street improvement recommendations and mitigation measures outlined in the subsequent traffic studies for each individual project.
- d. No textured pavement accents are allowed within city rights-of-way.
- e. Curb adjacent sidewalks shall maintain a minimum clear width of six feet (6'), except where there is a combination pedestrian/bike path where the minimum width is eight feet. All sidewalks shall be constructed of concrete. Vertical concrete curbs shall be provided for any curb which serves to carry storm runoff and shall be constructed with an integral gutter. All improvements shall conform to the City standards and specifications for road improvement.
- f. Cul-de-sac Standards – Cul-de-sacs shall be designed per standards set forth by City of Eastvale Ordinance.

Off-Street Parking Standards

- a. Parking Requirements – All required parking for uses within the *City of Eastvale I-15 Corridor Specific Plan* shall comply with the construction, dimension and quantity standards specified in the City of Eastvale Zoning Code. Provision of compact and handicapped parking stalls and bicycle parking facilities shall also be in accordance with the City of Eastvale Zoning Code.
- b. Parking Design Standards – All circulation, access, and design of parking areas within the *City of Eastvale I-15 Corridor Specific Plan* shall be consistent with applicable provisions of the City Parking Regulations.
- c. Landscaping/Screening – All parking and loading areas shall be visually buffered from residential areas and roadways by appropriate landscaping and screening. Parking areas shall be buffered from roadways to a minimum height of 30 inches and from residential areas to a minimum height of six feet (6'). Loading areas shall be screened from roadways and residential areas to a minimum height of six feet (6'). Screening may consist of berming, landscaping, fencing, walls, grade separations, or a combination of these. All parking lot landscaping shall be provided in accordance with the applicable provisions of the City Landscape Regulations.

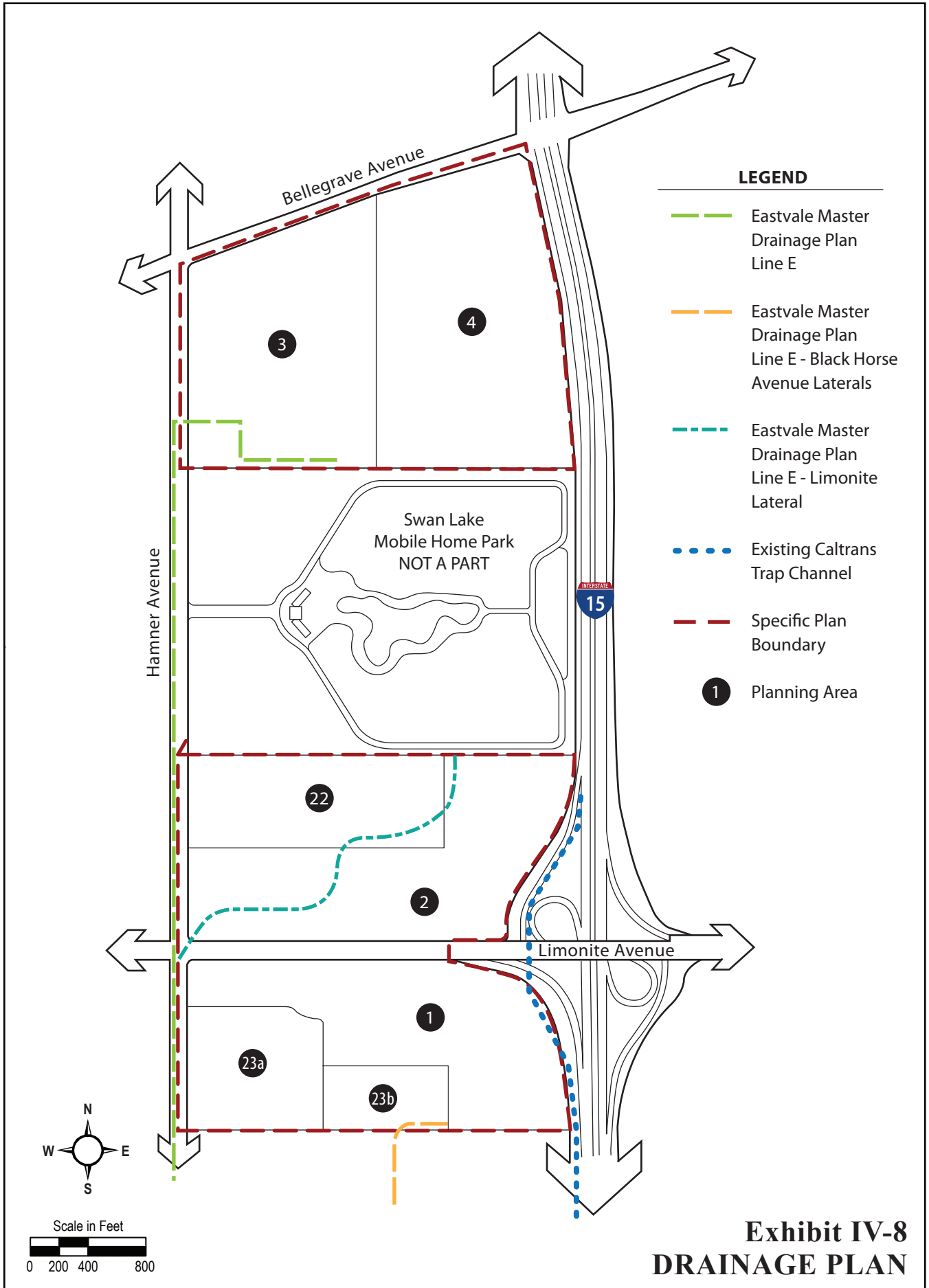
3. Drainage Plan

Drainage Facilities have been constructed as described below. All existing uses comply with the Drainage Development Standards and Guidelines set forth below. Only undeveloped Planning Areas (1 and 23b) shall be required to continue the enforcement of these Drainage Standards and Guidelines and develop accordingly, as indicated below.

The *City of Eastvale I-15 Corridor Specific Plan* is located within the Eastvale Area Drainage Plan. Under the plan, Master Drainage Plan (MDP) Line E has been constructed from the intersection of Hamner Avenue and Amberhill Avenue to the existing open channel located adjacent to Interstate-15, south of Kern River Drive. Stormwater runoff south of Bellegrave Avenue is collected by smaller public and private storm drain systems, inlets and catch basins and conveyed via Line E to the Santa Ana River.

The drainage plan for the *City of Eastvale I-15 Corridor Specific Plan* as reflected in **Exhibit IV-8, Drainage Plan**, shows the improvements of the Eastvale Area Drainage Plan and improvements constructed by Caltrans.

- a. All drainage and flood control measures are provided in accordance with the requirements of the Riverside County Flood Control and Water Conservation District, and consistent with the provisions of the Eastvale Area Drainage Plans as modified.
- b. All flood control facilities are developed in consistency with the mitigation measures herein.
- c. Storm drainage facilities ensure the acceptance and disposal of storm runoff without damage to streets or adjacent properties.
- d. All projects proposing construction activities including clearing, grading, or excavation that results in the disturbance of land, shall obtain the appropriate coverage under the National Pollutant Discharge Elimination System (NPDES) construction permit (State Water Resources Control Board – Order No. 2009-0009-DWQ) and pay the appropriate fees. All development within the Specific Plan boundaries shall be subject to future requirements adopted by the City to implement the NPDES program. Mitigation measures may include, but not be limited to, onsite retention; covered storage of all outside storage facilities; vegetated swales; monitoring programs, etc.



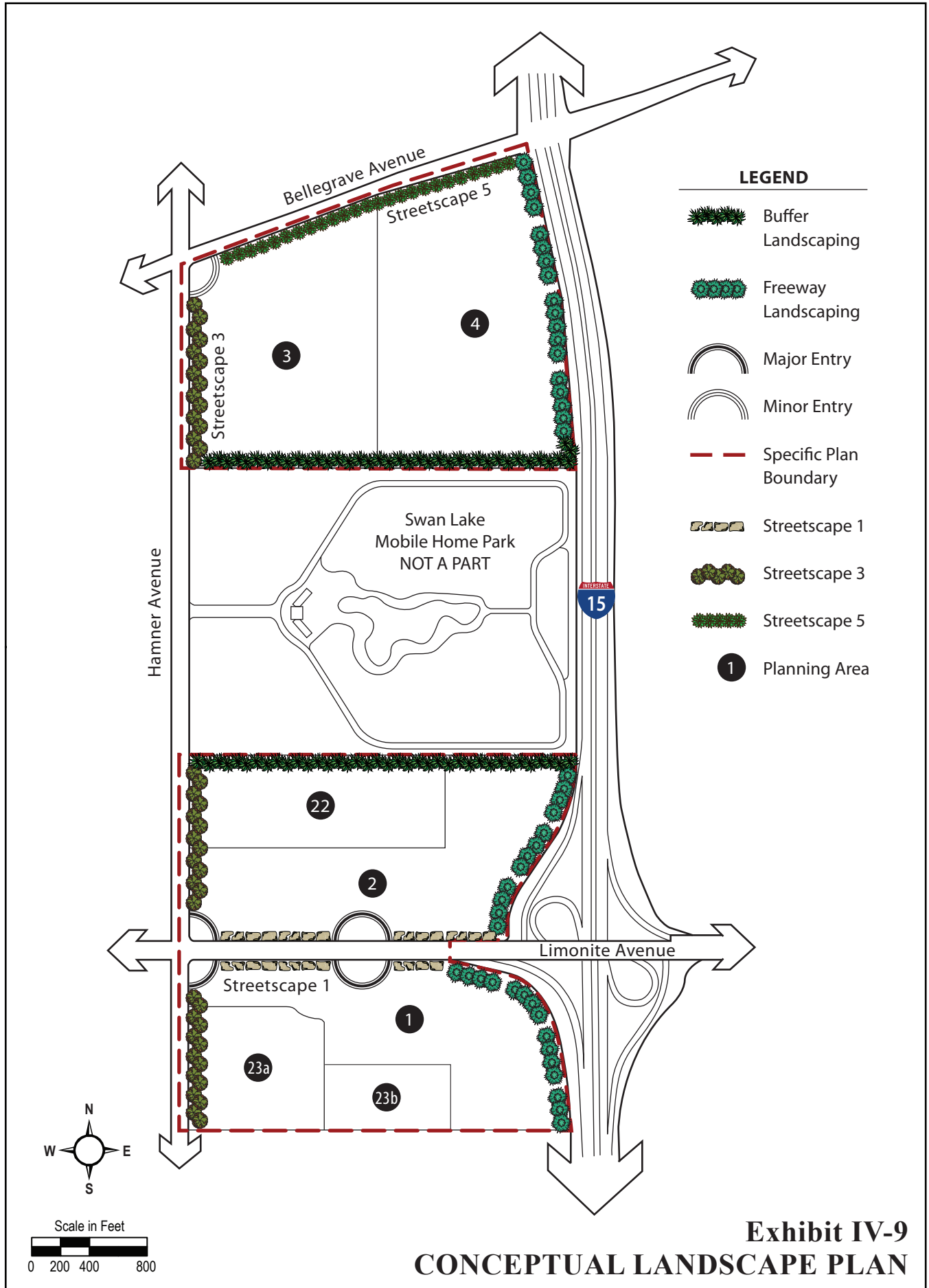
4. Landscape Plan

The conceptual Landscape Plan is a guideline to establish a theme and design criteria for the I-15 Corridor. The Landscape Plan has been designed to establish a project identity and graduated network of landscape corridors along this Specific Plan street system as reflected on **Exhibit IV-9, Conceptual Landscape Plan**. The conceptual Landscape Plan includes concepts for streetscapes and entry/intersection treatments. Various landscape treatments will be utilized to distinguish the hierarchy of streets.

The descriptions of the streetscape treatments can be found in Section IV.C, Design Guidelines.

Secondary entries are located at the intersections of Hamner and Limonite Avenues and the intersection of Hamner and Bellegrave Avenues. Major intersections within this Specific Plan occur along Limonite Avenue at the internal north/south connector and between Planning Areas 1 and 2.

The conceptual Landscape Plan for this Specific Plan is shown in **Exhibit IV-9. Exhibit IV-10, Parkway Plan**, is a parkway plan for the entire Specific Plan illustrating the three configurations for parkway treatments. **Exhibits IV-11 and IV-12, Parkway Sections**, illustrate the sections for the three types of parkway conditions. Detailed features and development standards of the landscape concept are described and illustrated in Section IV.C, Design Guidelines. Such features include walls and fences, streetscapes, entries, and open space treatments. The Design Guidelines also include landscape design standards and a plant palette.



LEGEND











-  Buffer Landscaping
-  Freeway Landscaping
-  Major Entry
-  Minor Entry
-  Specific Plan Boundary
-  Streetscape 1
-  Streetscape 3
-  Streetscape 5
-  Planning Area

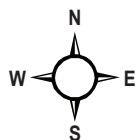
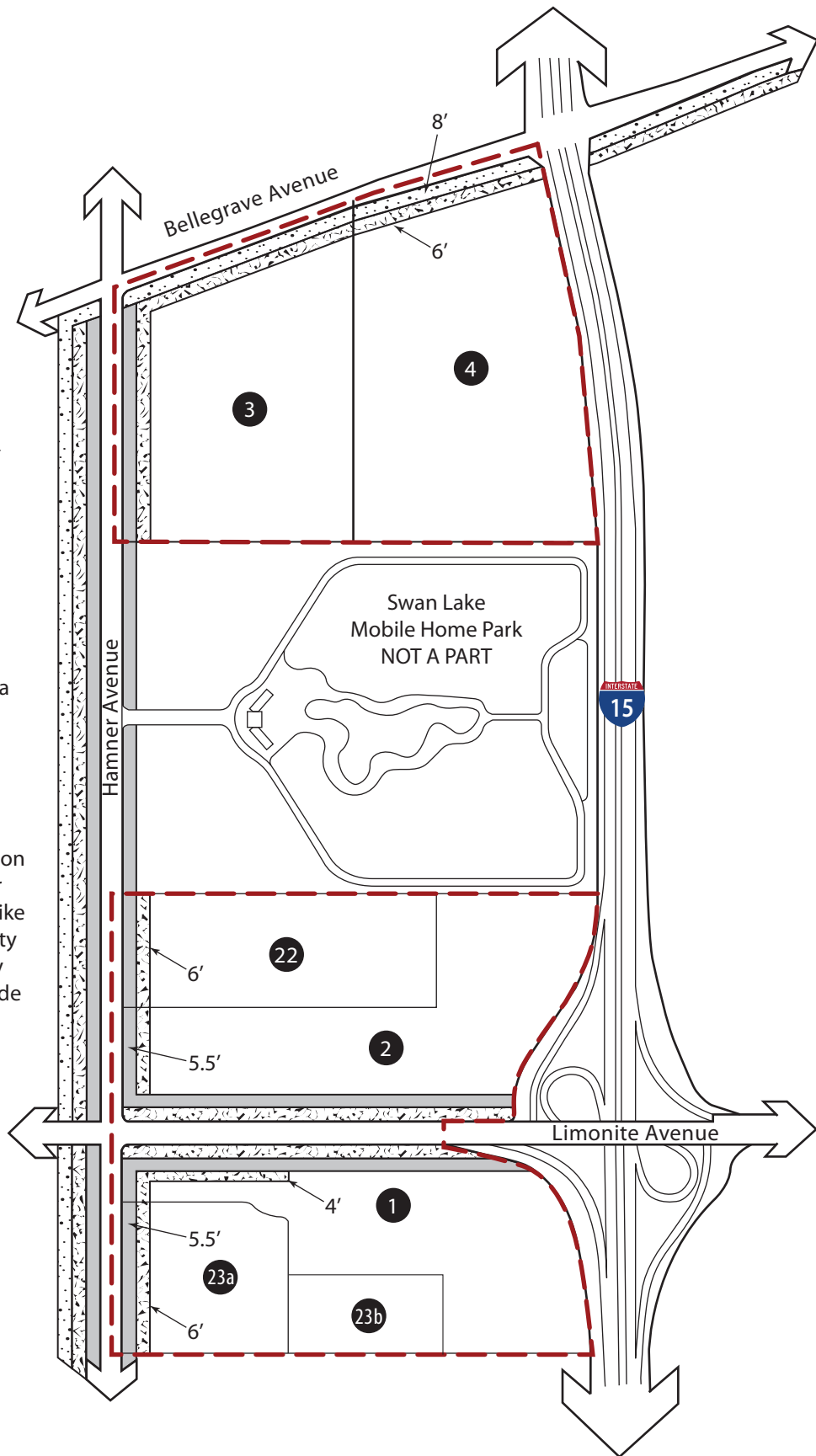
Exhibit IV-9

CONCEPTUAL LANDSCAPE PLAN

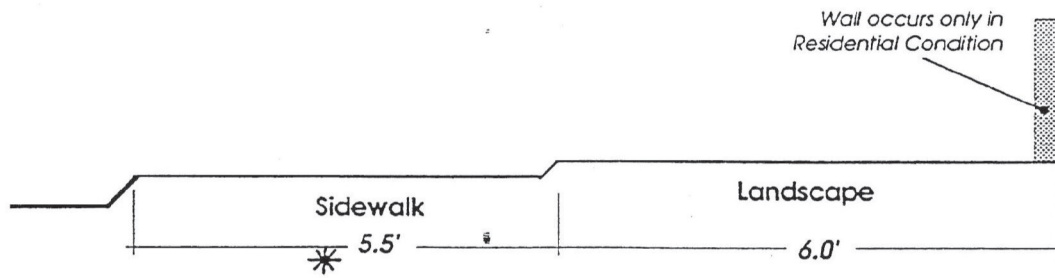
LEGEND

-  Landscaping
-  Sidewalk
-  Sidewalk/
Bike Trail
-  Planning Area
-  Specific Plan Boundary

Parkway Improvements on the west side of Hamner Avenue, including the bike trail, are the responsibility of the adjacent property owner along the west side of the street



**Exhibit IV-10
PARKWAY PLAN**



PARKWAY SECTION 1

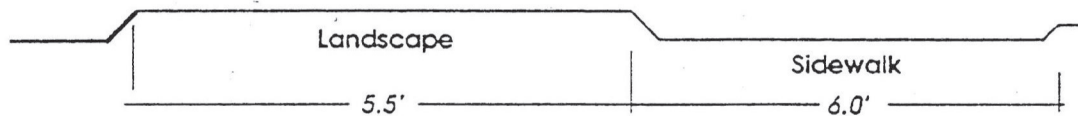
Exterior Streets - Residential Condition

- * South Side - Bellegrave Avenue (Planning Areas 3 and 4)
- East Side - Hamner Avenue (Planning Area 3, 23a and 23b)

- Commercial Condition

- East Side - Hamner Avenue (Planning Area 22)

* SOUTH SIDE BELLEGRAVE AVENUE
SIDEWALK / BIKE PATH 8' WIDE



PARKWAY SECTION 2

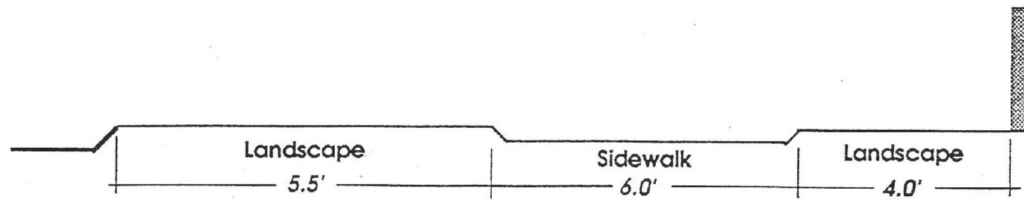
Interior Streets (Sidewalk) - Commercial Frontage

- North Side - Limonite Avenue (Planning Areas 2 and 22)
- South Side - Limonite Avenue (Planning Area 1)

NOT TO SCALE



**Exhibit IV-11
PARKWAY SECTIONS**



PARKWAY SECTION 3
Interior Streets (Sidewalk) - *Residential Frontage*

South Side - Limonite Avenue (Planning Areas 23a and 23b)

NOT TO SCALE

 **PLANNING AND DESIGN SOLUTIONS**
Community Planning • Urban Design • Environmental Services

ALBERT A.
WEBB
ASSOCIATES
ENGINEERING CONSULTANTS

Exhibit IV-12
PARKWAY SECTIONS

5. Water and Sewer Plan

Water and Sewer facilities have been constructed as described below. All existing uses comply with the Water and Sewer Development Standards and Guidelines set forth below. Only undeveloped Planning Areas (1 and 23b) shall be required to continue the enforcement of these Drainage Standards and Guidelines and develop accordingly, as indicated below.

a. Plan Descriptions

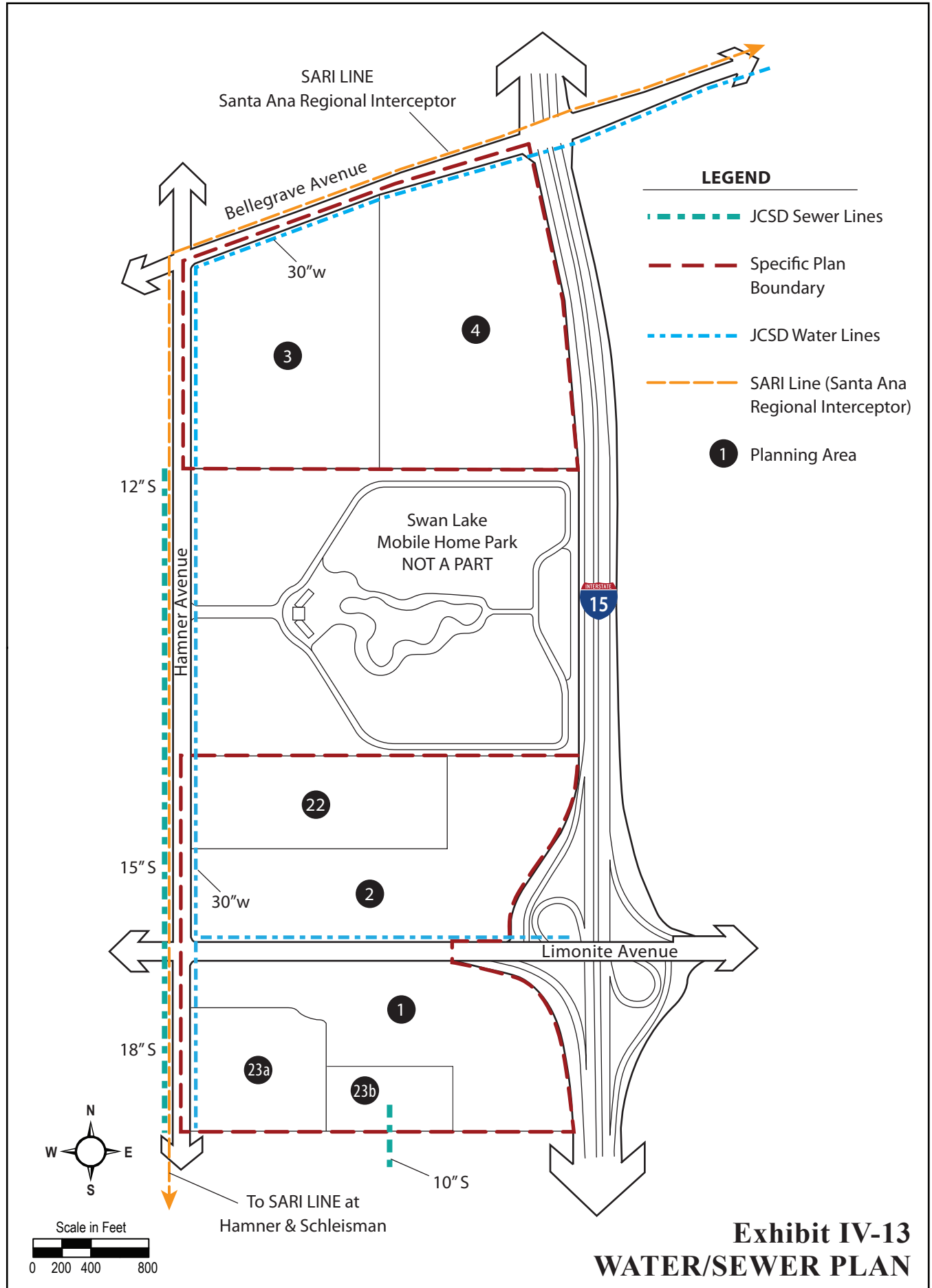
The *City of Eastvale I-15 Corridor Specific Plan* is located within the Jurupa Community Services District (JCSD), which is located within the Metropolitan Water District (MWD). The JCSD receives its water supply for this Specific Plan area from wells within the Chino Groundwater Basin. Adequate infrastructure has been constructed to provide water service to this Specific Plan. A water main has been connected to the existing system northeast of this Specific Plan area, and extended along Bellegrave Avenue to Etiwanda Avenue. Within the Specific Plan area, 30-inch water mains have been installed along Limonite, Hamner and Bellegrave Avenues, and along a north-south route between Bellegrave and Limonite Avenues. The existing water facilities which provide water service to this Specific Plan area are shown in **Exhibit IV-13, Water and Sewer Plan**.

Sewer service for this Specific Plan is provided by JCSD. JCSD has treatment capacity within the Western Riverside County Regional Wastewater Reclamation Facility (WRCRWF) located west of River Road, just north of the Santa Ana River. Within this Specific Plan, sewer lines range from 12 inches to 18 inches in diameter, and are located within Hamner Avenue. Sewage from this Specific Plan area is conveyed to the WRCRWF via JCSD owned and maintained sewer facilities. The existing sewer facilities within the Specific Plan area are shown in **Exhibit IV-13, Water/Sewer Plan**, below.

b. Development Standards

- All water and sewer lines shall be placed underground.
- All water and sewer lines shall be installed in accordance with the requirements and specifications of JCSD.
- Water and sewage facilities shall be installed in accordance with the requirements and specifications of the City of Eastvale Public Works Department.
- It is the intention of the developer to participate in any regional water supply program at the time development occurs, and should existing JCSD water supply facilities be inadequate to provide service, the developer will participate in the construction and financing of required improvements.
- Flow metering stations, lift stations, force mains, and trunk sewers required to provide sewer service to this Specific Plan have been constructed.

- The developer shall participate in any future reclaimed water system that would be provided by JCSD. The District currently does not provide reclaimed water as a service. Installation of future facilities would be pursuant to District design standards.



**Exhibit IV-13
WATER/SEWER PLAN**

6. Public Facilities

Public facilities for existing uses have been constructed per the Development Standards and Guidelines below. In addition, park requirements were accomplished through the larger Specific Plan document referred to as COR SP266. Only undeveloped planning areas (1 and 23b) shall be required to continue the enforcement of these Public Facilities Standards and Guidelines and develop accordingly, as indicated below.

Public services such as fire and police protection, solid waste management, libraries, and health care facilities are provided for through payment of applicable development mitigation fees.

The provisions for school and park facilities have been made through a combination of land reservations, land dedications, fees, and improvements. Details regarding the provision and adequacy of public services are included in Environmental Impact Report No. 340, Addendum No. 2.

a. Public Facility Standards

- 1) All utility lines shall be underground. No pipe, conduit, cable, line of water, gas, sewage, drainage, electricity, or any other energy or service shall be installed or maintained upon any lot (outside of any building) above the surface of the ground, except for hoses, movable pipes used for irrigation, or other purposes during construction of transformers. Major flood control improvements may require open channels.
- 2) The development shall conform to state and local requirements for energy conservation.
- 3) The developer shall mitigate, in accordance with state law and City regulations, project-related public facilities and/or school district(s) impacts which may be identified at the tentative tract or building permit stage for this Specific Plan.
- 4) In the event consideration is given to the formation of a Community Facilities District (CFD), or other funding mechanism to provide maintenance and/or other public services for this area, the area covered by this Specific Plan shall be considered for inclusion in this funding program.
- 5) Fire protection shall be provided in accordance with the applicable standards set forth by City of Eastvale Ordinance, throughout this entire Specific Plan.

b. Comprehensive Funding Plan

The ability to fund public facilities necessary for support of development is one of the most challenging aspects of the planning and development process. The overall approach in the *City of Eastvale I-15 Corridor Specific Plan* has been to use one or more of the basic funding approaches indicated to phase logical and affordable increments of the public facilities and services in conjunction with appropriate components of development. The widest possible array of funding methods is suggested because of the scale of improvements and the desire to not unreasonably escalate the cost of housing, so as to slow market absorption rates to a point at which development would not be feasible.

1) Basic Funding Approaches**Assessment Districts**

The Municipal Improvement Act of 1913 (Streets and Highways Code Section 10000 et seq.) is a procedural act which provides for the formation of a special assessment district, the levy of an assessment, and the creation of a lien against the property, but does not, in itself, contain provisions for the issuance of bonds. For this reason, bonds are issued under the 1911 Act or 1915 Act, discussed below. An assessment district is created for construction of acquisition of a wide variety of public improvements.

Under the 1913 Act, a proposed assessment and assessment diagram are prepared before any construction work is done. The amount of the assessment levied against each property is based upon an engineer's report and notices are sent to property owners. A public hearing is held, both upon the project and the amount of the proposed assessment to be levied. At the conclusion of the public hearing, the governing body may confirm the assessments. If confirmed, a lien is created against each assessed parcel and the assessments are properly recorded. The property owners are mailed notices of each parcel's exact confirmed assessments and they have 30 days to pay a part or all of the assessment in cash. Upon conclusion of the cash collection period, all unpaid assessments are accumulated, a bond issue is structured and bonds are sold. The bonds may be issued pursuant to either the Improvement Act of 1911 (Streets and Highways Code Section 5000 et seq.) or the Improvement Bond Act of 1915 (Streets and Highways Code Section 8500 et seq.).

Each bond issued under the 1911 Act constitutes a direct lien against a specific piece of property. This lien is on a parity with general taxes and takes priority over any private lien such as a deed of trust, mortgage, or attachment. In the event the property owner fails to pay an installment on or prior to its delinquency date, penalties accrue against the delinquent amount, and such penalties are paid to the bondholder upon

redemption of the delinquent installment or installments. If redemption of a delinquency is not made, the bondholder may institute the foreclosure action.

Under the 1915 Act, individual bonds are not issued to represent individual assessments. Bonds equaling the total unpaid assessment are issued in even denominations and a portion of the bonds are due each year for the life of the issue which will result in approximately equal annual principal and interest payments. The assessment liens securing the bonds are payable in installments, which in the aggregate conform to the principal and interest payments on the bonds. Assessment installments appearing on the regular property tax bill are collected in the same time and in the same manner as property taxes, and bear the same penalties for delinquency including the sale of the property at public auction. A special reserve fund is available from which to make payment to the bondholder of any delinquent assessments which might occur, and foreclosure proceedings, similar to those in the 1911 Act, are also available. These liens are also superior to other deeds of trust and similar liens.

Conventional Financing

Conventional financing, in which major infrastructure improvements were financed by municipalities through property taxes, has historically been the norm in California development. Proposition 13 changed that. It removed a significant portion of the property tax that used to produce local revenues from new developments. This money had been used to service the City's facilities and provide protective services. Interest rates on conventional loans for facilities have become too high to make it feasible in many cases. Due to current interest rates and the amount of money that must be financed, the main investors who can afford to install the needed infrastructure are those with access to large assets, such as banks, savings and loans, and insurance companies. Cities now must find ways to finance the facilities and services, due to the limitations on property tax revenue. To a certain extent, the costs of providing facilities and services can be offset by exactions and/or fee programs enforced on developers. These costs, which are increasing all the time, are frequently too great for the developer or jurisdiction. Therefore, other creative financing mechanisms may come into play.

In the past, bond issues were the simplest and most often utilized method of raising large amounts of capital. Because of changing conditions in the financial marketplace, and increased legislation limiting the bonding power of many local governments, the role of the traditional general obligation bond has been substantially reduced. Other bonding alternatives include:

- Revenue Bond
- Lease-Revenue Bond

- Zero Coupon Bond
- Stepped Coupon Bond

These techniques have historically been used for redevelopment and, therefore, would probably have limited application in the Specific Plan development.

Leasing is another financing mechanism that has been used by cities in the past. Leasing is used primarily for purchasing equipment and not for providing services, but it could be used for public buildings in conjunction with other development projects. A few leasing techniques are listed below:

- Leveraged Lease
- Operating Lease
- Lease-Purchase Agreement

Community Facilities Districts

The Mello-Roos Community Facilities Act of 1982, commencing with Section 53311 of the Government Code, allows a local public agency to form a "community facilities district" within its boundaries to provide certain specified public improvements and services for the benefit of the present and future residents within the community facilities district. Proceedings for the formation of a community facilities district may be instituted by the governing body of a public agency on its own initiation, and shall be instituted when the requisite number of registered voters or landowners within the proposed community facilities district's boundaries file a petition requesting the institution of such proceedings with the clerk of the governing body.

Proceedings will commence with the public agency's adoption of a Resolution of Intention to form a community facilities district and levy a "special tax," and where applicable, a Resolution of Intention to incur bonded indebtedness. The public agency then holds a duly noticed public hearing on the proposed formation of the community facilities district to consider the facilities to be constructed and the services to be provided, the incurring of bonded indebtedness, and the levy of a "special tax" to pay the debt service on any bonds subsequently issued.

At the hearing, the governing body of the public agency receives testimony from the staff and from all interested taxpayers, including the petitioning landowner. After receiving testimony, if the governing body determines to proceed with forming the district and calling for an election on the propositions of authorizing the indebtedness and levying the special tax within the boundaries of the community facilities district, a duly noticed election is then held within the community facilities district on the propositions of authorizing bonded indebtedness and levying the special tax. If the

propositions are approved by two-thirds of the voters, the governing body may adopt resolutions relative to issuing the bonds, approving the Official Statement, and approving the other miscellaneous matters required to sell the bonds.

The special tax, which is levied subsequent to any bond issuance for the purpose of paying debt service on such bonds, is not in the nature of an assessment, it is not capable of being prepaid and there is not a fixed, determinable amount against each parcel. The actual amount of the special tax against each parcel will require minor adjustments each year depending upon the extent to which interest earned on the construction fund or the bond reserve fund is applied towards debt service, the extent to which state funding is received, and changes in the plan for development of the property. The special tax will be collected on the tax rules of the City as any other tax of a special district. The special tax levied for each fiscal year becomes a lien for taxes against a particular parcel and is on a parity with the other tax liens and assessments.

The Act authorizes the public agency to collect delinquencies in the payment of the special tax in the same manner as delinquencies in the payment of ad valorem taxes. However, the Act also authorizes the public agency to adopt an alternative for the collection of any such delinquencies. Such a procedure could include a judicial foreclosure.

The Act is particularly appropriate for providing a means of financing certain regional improvements which may be necessary for the development of a particular property, as well as traditional local facilities.

There are other mechanisms available for funding schools in addition to those mentioned in the above discussion. Included as possible financing solutions are bonds, assessment districts, special taxes, impact fees, non tax local options, lease-purchase options, and other non- construction alternatives, such as year round education.

Eligible Improvements

Improvements eligible for the above-described funding mechanisms shall be those improvements determined to be of regional or area-wide benefit. In the case of this Specific Plan, many of the infrastructural improvements benefit undeveloped properties outside the boundary of this Specific Plan. Such improvements include, but are not limited to, the following:

- I-15/Limonite Interchange Improvements
- Regional Multi-purpose Trail
- Regionally-sized Water Lines
- Regionally-sized Sewer Lines

- Regionally-sized Drainage Facilities
- Hamner Avenue
- Limonite Avenue
- Bellegrave Avenue

7. Grading Plan

This section describes the overall grading concept utilized in the topographic modification of properties within the *City of Eastvale 1-15 Corridor Specific Plan*. The conceptual grading plan is shown in **Exhibit IV-14, Illustrative Grading Plan**. No import or export of dirt within this Specific Plan has been anticipated. In areas impacted by excessive amounts of manure, the manure will be removed and deposited at a pre-approved location pursuant to City of Eastvale Public Works Department.

Grading Standards

- a. Grading shall be in general conformance with the overall illustrative grading plan.
- b. The applicant and/or developer shall be responsible for the maintenance and upkeep of all slope planting and irrigation systems until such time as these operations are the responsibility of other parties.
- c. Graded slopes, if any, shall be oriented to minimize visual impacts to surrounding areas.
- d. Project grading will be designed to implement efficient drainage patterns consistent with the Eastvale Area Drainage Plan.
- e. Prior to any development within a planning area, a conceptual grading plan for the planning area shall be submitted for City Planning Department approval. The conceptual grading plan shall be used as a guideline for subsequent detailed grading plans for individual phases of development within that project and shall include the following:
 - 1) Techniques to prevent erosion and sedimentation during and after the grading process.
 - 2) Approximate time frames for grading including identification of areas which may be graded during the higher probability rain months of January through March.
 - 3) Preliminary pad and roadway elevations.
- f. No grading shall be permitted prior to issuance of grading permits.
- g. Residential development shall be graded so as to direct drainage from backyard to front yard where feasible. Cross lot drainage is not allowed, but may be permitted in unique situations with prior approval from the City.
- h. To limit erosion, grading shall be phased and limited as much as possible. Watering and re-landscaping will be used to limit air impact.
- i. Manufactured slopes, other than lined drainage channels or retained side slopes, should be no steeper than 2:1.
- j. Roads should be graded to a finished grade of no more than 15 percent.

- k. Any manufactured slopes greater than 10 feet may be allowed with prior approval from the City Planning Department if they are recommended to be safe in a slope stability report from a soil engineer.
- l. The tops and toes of slopes greater than 10 feet shall be rounded with curves that have radii in proportion to the total height of the slope.

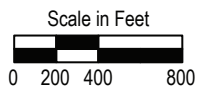
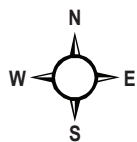
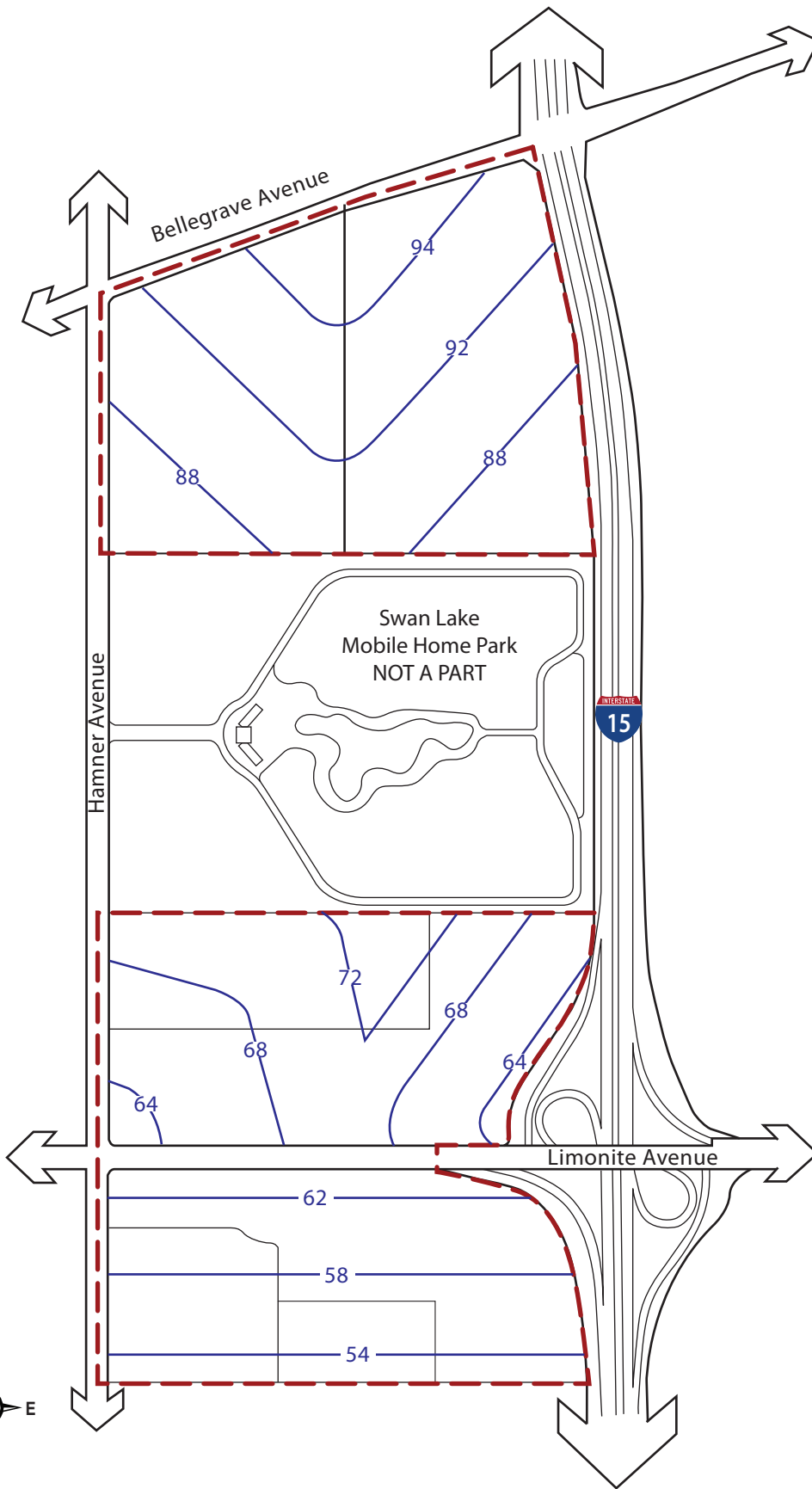


Exhibit IV-14
ILLUSTRATIVE GRADING PLAN

8. Comprehensive Maintenance Plan

In order to provide for the long-term maintenance of various areas throughout the *City of Eastvale I-15 Corridor Specific Plan*, a series of maintenance responsibilities have been assigned (or will be assigned for undeveloped planning areas) to a variety of associations, agencies or districts. Included in the possible responsible parties are a Community Facilities District, Landscape and Lighting Maintenance District, Master and Sub-Homeowners Associations, Commercial Associations, City of Eastvale Public Works Department, Jurupa Community Service District and the local school district.

A brief description of each responsible party is provided below and followed by a matrix, as depicted on **Table IV-3, Maintenance Responsibility Matrix**, which illustrates the organizations accountable for each type of maintenance responsibility. This is a conceptual representation of a possible maintenance framework. Final establishment of maintenance responsibility will be made prior to the approval of the Tentative Tract Map or Site Plan review as applicable.

- a. Community Facilities District – A permanent Community Facilities District (CFD) may be established to own and maintain common areas within the Specific Plan. The CFD would retain responsibility for all common recreation, open space, landscaped areas and street lighting.
- b. Homeowners Associations – A Homeowners Association may be designated to own and maintain certain common areas.
- c. Commercial Associations – A separate association may be established to maintain common areas in commercial developments. Commonly, such things as parking lots, landscaping, signage and lighting are included in the maintenance responsibility of the Commercial Association. Such an association will not be required where the commercial center is under sole ownership.
- d. Open Space, Trails and Parks – Open space areas, private trails and parks will be owned and maintained by either a CFD or a Homeowners Association. Public trails such as the equestrian trail or bicycle trails in the public right-of-way would be maintained by the City.
- e. Roads – Maintenance for public roadways through this Specific Plan will be the responsibility of the City of Eastvale Public Works Department, as approved by the City Council, and as approved by Caltrans in the case of I-15. Private roadways will be maintained by a Homeowners Association or sole owners.
- f. School Sites – School sites have been financed, constructed and maintained by the appropriate school district, subsequent to site acquisition.

If maintenance is to be performed by private associations, these associations will be established through the following procedures:

- 1) Prior to recordation of any final land division map, which includes property that will be owned and maintained by an association, the applicant shall submit to the Planning Department the following documents which shall demonstrate to the satisfaction of the City that the appropriate owners' associations will be established and will operate in accordance with the intent and purpose of the Specific Plan:
 - a) The document to convey title.
 - b) Covenants, Conditions and Restrictions (CC&Rs) shall be recorded at the same time that each final subdivision map is recorded.
 - c) The CC&Rs shall be structured to include the following provisions:

The master property owners' association and commercial owners' association shall be charged with the unqualified right to assess individual owners of individual units for reasonable maintenance and management costs which shall be established and continuously maintained. The owners' associations shall have the right to lien the property of any owners who default in payment of their assessment fees. Such lien shall not be subordinate to any encumbrance other than a first deed of trust, provided such deed of trust is made in good faith and for good value, and is of record prior to the lien of the individual owners' association.

Table IV-3, Maintenance Responsibility Matrix

	Community Facilities District	Sub H.O. Association	Individual Property Owner	Commercial Association	School District	City of Eastvale	Caltrans	Utility Company
Public Streets (Curb to Curb Including Medians)						○		
Private Streets (Curb to Curb Including Medians)		*	*	*				
Interstate-15							○	
Parkway Landscaping (Public Street)	○					○		
Parkway Landscaping (Private Street)		*	*	*				
Street Lighting (Public Street)	○					○		
Street Lighting (Private Street)	*	*	*	*				
Sidewalk (Public Street)						○		
Sidewalk (Private Street)	*	*	*	*				
Sidewalk (Internal)		*	*	*				
Street Signage (Public Street)						○		
Street Signage (Private Street)		*	*	*				
Multi-Use Trail						○		
Bike Ways						○		
Storm Drains (Public Street)						○		
Storm Drains (Private Street)	*	*	*					
Open Space Areas	*	*	*					
Landscape Areas	*	*	*	*				
Landscape Easements	*							

	Community Facilities District	Sub H.O. Association	Individual Property Owner	Commercial Association	School District	City of Eastvale	Caltrans	Utility Company
Slope Areas	*	*	*	*				
Park (Jurupa Community Parks and Recreation District)						○		
Recreational Facilities (Private)		*	*					
School					○			
Parking (Off Street)		*	*	*				
Signage (Project)		*	*	*				
Lighting (Project)		*	*	*				
Water								○
Sewer								○
Gas								○
Electric								○
Telephone								○
Cable TV								○
Fire						○		
Police						○		
Library						○		

○ Indicates Responsible Agency

* To be determined at Tentative Map/Site Plan Stage

9. Transportation Demand Management Plan

The transportation management plan is provided to assist future residents, and commercial and industrial tenants, to comply with the policies and programs of the Air Quality Implementation Program (AQIP) adopted by the Western Riverside Council of Governments (WRCOG). The AQIP (Phase I) has been developed by the WRCOG in order to comply with the South Coast Air Quality Management District's air quality management plan and avoid risks to future federal infrastructure funding which will be critical for regional circulation improvements.

All developments in the Specific Plan have incorporated (or shall incorporate for undeveloped planning areas) the following applicable transportation management measures, which are subject to approval and monitoring as required by a transportation management agency. New employment generating developments that could employ 100 or more employees shall submit trip reduction plans that reduce work related trips by 12 percent. Trip reduction plans shall be based on existing guidelines at the time occupancy permits are pulled.

Residential

- 1) Information center for transportation alternatives
- 2) Bus stop improvements

Commercial

- 1) Preferential parking for carpool and vanpool vehicles
- 2) Bicycle parking and shower facilities
- 3) Information center for transportation alternatives
- 4) Rideshare vehicle loading areas
- 5) Bus stop improvements

Industrial

- 1) Preferential parking for carpool and vanpool vehicles
- 2) Bicycle parking and shower facilities
- 3) Information center for transportation alternatives
- 4) Rideshare vehicle loading areas
- 5) Bus stop improvements
- 6) On-site child care improvements
- 7) Local transportation systems management
- 8) Telecommuting facilities
- 9) On-site service amenities including food and bank teller services

B. PLANNING AREA DEVELOPMENT STANDARDS

1. Planning Area 1 – Commercial-Retail

a. Descriptive Summary

Planning Area 1 is proposed as Commercial-Retail (CR). This designation is intended to provide an area for uses which compliment the regional complex in Planning Area 2. Typical uses would include support and specialty retailers, automotive services, and restaurants. The gross area of Planning Area 1 is 45.0 acres.

b. Land Use and Development Standards

This Planning Area is zoned Specific Plan (SP) and Development Standards in this Planning Area shall be the same as the Development Standards of the Scenic Highway Commercial (C-P-S) Zone contained in the most recent version of the City of Eastvale Zoning Code that is in effect at the time a project is submitted.

c. Planning Standards

- 1) Primary access is gained by one full intersection and two right-in/right-out intersections on Limonite Avenue, as depicted in **Exhibit IV-4, Access and Control Plan**.
- 2) Secondary access is gained by one right-in/right-out on Hamner Avenue, as depicted in **Exhibit IV-4, Access and Control Plan**.
- 3) Perimeter landscape treatments shall be in accordance with the Conceptual Landscape Plan, Section IV.A.4, and as depicted in **Exhibit IV-9, Conceptual Landscape Plan**.
- 4) Intersection monumentation, as depicted in **Exhibit IV-9, Conceptual Landscape Plan** and **Exhibit IV-21, Major Entry Plan** shall be located on Limonite Avenue midway between I-15 and Hamner Avenue.
- 5) Perimeter fencing for Planning Area 1 shall be provided in accordance with the Wall and Fence Plan depicted in **Exhibit IV-29, Wall and Fence Plan**.
- 6) For Specific Plan Design Guidelines including general, landscape and architectural Design Guidelines, please refer to Section IV.C.

2. Planning Area 2 – Commercial-Retail

a. Descriptive Summary

Planning Area 2 is Commercial-Retail (CR). This designation provides for a regional retail and business complex. Typical uses include a shopping mall, offices, restaurants and hotels. The gross area of Planning Area 2 is 50.4 acres.

b. Land Use and Development Standards

This Planning Area is zoned Specific Plan (SP) and Development Standards in this Planning Area shall be the same as the Development Standards of the Scenic Highway Commercial (C-P-S) Zone contained in the most recent version of the City of Eastvale Zoning Code that is in effect at the time a project is submitted.

c. Planning Standards

- 1) Primary access is gained by one full intersection and two right-in/right-out intersections on Limonite Avenue and one full intersection on Hamner Avenue (shared with Planning Area 22), as depicted in **Exhibit IV-4, Access and Control Plan**.
- 2) Perimeter landscape treatments are in accordance with the Conceptual Landscape Plan, Section IV.A.4, and as depicted in **Exhibit IV-9, Conceptual Landscape Plan**.
- 3) Intersection monumentation, as depicted in **Exhibit IV-9, Conceptual Landscape Plan** and **Exhibit IV-21, Major Entry Plan**, is located on Limonite Avenue midway between I-15 and Hamner Avenue.
- 4) Perimeter fencing for Planning Area 2 is in accordance with the Wall and Fence Plan depicted in **Exhibit IV-29, Wall and Fence Plan**.
- 5) For Specific Plan Design Guidelines including general, landscape and architectural Design Guidelines, please refer to Section IV.C.

3. Planning Area 3 – Medium-High Density Residential

a. Descriptive Summary

Planning Area 3 is Medium-High Density Residential (MHDR). This designation provides for single-family dwelling units. The maximum density is 4.8 du/ac. Typical uses include single-family detached or attached units. The gross area of Planning Area 3 is 50.8 acres. The maximum number of units allowed in Planning Area 3, subject to transfer of density provisions, is 244 units.

b. Land Use and Development Standards

Development Standards in this Planning Area shall be the same as the Development Standards of the Multiple Family Dwellings (R-2) Zone contained in the most recent version of the City of Eastvale Zoning Code that is in effect at the time a project is submitted.

c. Planning Standards

- 1) Primary access is gained by one full intersection on Hamner Avenue, as depicted in **Exhibit IV-4, Access and Control Plan**.
- 2) Secondary access is gained by an access drive on Hamner Avenue, a full intersection on Bellegrave Avenue and an access point from Planning Area 4, as depicted in **Exhibit IV-4, Access and Control Plan**.
- 3) Perimeter landscape treatments are in accordance with the Conceptual Landscape Plan, Section IV.A.4, and as depicted in **Exhibit IV-9, Conceptual Landscape Plan**.
- 4) Entry monumentation, as depicted in **Exhibit IV-9, Conceptual Landscape Plan** and **Exhibit IV-23, Secondary Entry Plan**, is located at the northwest corner of the Planning Area at the intersection of Hamner Avenue and Bellegrave Avenue (secondary).
- 5) A bike lane, as depicted in **Exhibit IV-2, Open Space and Recreation Plan**, is provided along the south side of Bellegrave Avenue.
- 6) Perimeter fencing for Planning Area 3 is in accordance with the Wall and Fence Plan depicted in **Exhibit IV-29, Wall and Fence Plan**.
- 7) For Specific Plan Design Guidelines including general, landscape and architectural Design Guidelines, please refer to Section IV.C.

4. Planning Area 4 – High Density Residential

a. Descriptive Summary

Planning Area 4 is High Density Residential (HDR). This designation provides for multi-family dwelling units. The maximum density is 12 du/ac. Typical uses include condominiums or apartments. This type and intensity of development is warranted due to its proximity to the freeway and proposed industrial development to the north. The effects of this type of development on surrounding areas are severely limited because the site is bounded by the freeway and on the north by Bellegrave Avenue. Industrial uses are proposed on the north side of Bellegrave Avenue. The gross area of Planning Area 4 is 61.5 acres. The maximum number of dwelling units in Planning Area 4, subject to transfer of density provisions, is 738 units.

b. Land Use and Development Standards

Development Standards in this Planning Area shall be the same as the Development Standards of the General Residential (R-3) Zone contained in the most recent version of the City of Eastvale Zoning Code that is in effect at the time a project is submitted.

c. Planning Standards

- 1) Primary access is gained by one full intersection on Bellegrave Avenue, as depicted in **Exhibit IV-4, Access and Control Plan**.
- 2) Secondary access is gained by an access point from Planning Area 3, as depicted in **Exhibit IV-4, Access and Control Plan**.
- 3) Perimeter landscape treatments are in accordance with the Conceptual Landscape Plan, Section IV.A.4, and as depicted in **Exhibit IV-9, Conceptual Landscape Plan**.
- 4) Perimeter fencing for Planning Area 4 is in accordance with the Wall and Fence Plan depicted in **Exhibit IV-29, Wall and Fence Plan**.
- 5) For Specific Plan Design Guidelines including general, landscape and architectural Design Guidelines, please refer to Section IV.C.
- 6) A bike lane, as depicted in **Exhibit IV-2, Open Space and Recreation Plan**, is provided along the south side of Bellegrave Avenue.

5. Planning Area 22 – Commercial-Retail

a. Descriptive Summary

Planning Area 22 is Commercial-Retail (CR). This designation provides for a regional retail and business complex. Typical uses include a shopping mall, offices, restaurants and hotels. This Planning Area has been developed in conjunction with Planning Area 2. The gross area of Planning Area 22 is 25.9 acres.

b. Land Use and Development Standards

This Planning Area is zoned Specific Plan (SP) and Development Standards in this Planning Area shall be the same as the Development Standards of the Scenic Highway Commercial (C-P-S) Zone contained in the most recent version of the City of Eastvale Zoning Code that is in effect at the time a project is submitted.

c. Planning Standards

- 1) Primary access is gained by one full intersection on Hamner Avenue (shared with Planning Area 2), as depicted in **Exhibit IV-4, Access and Control Plan**.
- 2) Secondary access is gained by right-in/right-out intersection on Hamner Avenue, as depicted in **Exhibit IV-4, Access and Control Plan**.
- 3) Perimeter landscape treatments are in accordance with the Conceptual Landscape Plan, Section IV.A.4, and as depicted in **Exhibit IV-9, Conceptual Landscape Plan**.
- 4) Perimeter fencing for Planning Area 22 is in accordance with the Wall and Fence Plan depicted in **Exhibit IV-29, Wall and Fence Plan**.
- 5) For Specific Plan Design Guidelines including general, landscape and architectural design guidelines, please refer to Section IV.C.

6a. Planning Area 23a – High Density Residential

a. Descriptive Summary

Planning Area 23a is High Density Residential (HDR). This designation provides for multi-family dwelling units. The maximum density is 15.9 du/ac. Typical uses include condominiums or apartments. This type and intensity of development is warranted due to its proximity to proposed commercial development to the north. The gross area of Planning Area 23a is 19.7 acres. The maximum number of dwelling units in Planning Area 23a, subject to transfer of density provisions, is 314 units.

b. Land Use and Development Standards

Planning Area 23a is zoned Specific Plan (SP) and the Development Standards in this Planning Area shall be the same as the Development Standards of the General Residential (R-3) Zone contained in the most recent version of the City of Eastvale Zoning Code that is in effect at the time a project is submitted.

c. Planning Standards

Planning Area 23a

- 1) Primary access is gained by one full intersection on Hamner Avenue, and one full intersection taking access through Planning Area 1 as depicted in **Exhibit IV-4, Access and Control Plan**.
- 2) Secondary access is gained by one-right-in/right-out intersection on Hamner Avenue, as depicted in **Exhibit IV-4, Access and Control Plan**.
- 3) Perimeter landscape treatments for Planning Area 23a are in accordance with the Conceptual Landscape Plan, Section IV.A.4, and as depicted in **Exhibit IV-9, Conceptual Landscape Plan**.
- 4) No buildings are constructed that exceed two stories in height unless elevators are included for the use of the occupants. Residential buildings exceeding two stories provide additional elevators if they are needed due to the number of units or project design. Elevators are placed in locations which minimize the walking distance from the elevators to the residential units.
- 5) Perimeter fencing for Planning Area 23a is in accordance with the Wall and Fence Plan depicted in **Exhibit IV-29, Wall and Fence**.
- 6) For Specific Plan Design Guidelines including general, landscape and architectural Design Guidelines, please refer to Section IV.C.

- 7) Garage setbacks in Planning Area 23a are a minimum of three feet (3') for interior streets and drives. All other building setbacks for interior streets and drives are a minimum of five feet (5'). Building setbacks from exterior boundary lines are five feet (5').

6b. Planning Area 23b – Highest Density Residential

a. Descriptive Summary

Planning Area 23b is proposed as Highest Density Residential (HHDR). This designation is intended to provide an area for the development of multi-family dwelling units. The maximum overall density is 30.0 du/ac. Typical uses would include condominiums or apartments. This type and intensity of development is warranted due to its proximity to proposed commercial development to the north and east. The gross area of Planning Area 23b is 10 acres. The maximum number of dwelling units in Planning Area 23b, subject to transfer of density provisions, is 300 units.

b. Land Use and Development Standards

Planning Area 23b is zoned Specific Plan (SP) and Development Standards in this Planning Area shall be the same as the Development Standards of the General Residential (R-3) Zone contained in the most recent version of the City of Eastvale Zoning Code that is in effect at the time a project is submitted.

c. Planning Standards

- 1) Primary access is gained by one full intersection on Limonite Avenue taking access through Planning Area 1 as depicted in **Exhibit IV-4, Access and Control Plan**.
- 2) Secondary access is gained by one-right-in/right-out intersection on Hamner Avenue, as depicted in **Exhibit IV-4, Access and Control Plan**.
- 3) Perimeter landscape treatments for Planning Area 23b shall be in accordance with the Conceptual Landscape Plan, Section IV.A.4, and as depicted in **Exhibit IV-9, Conceptual Landscape Plan**.
- 4) No buildings shall be constructed that exceed two stories in height unless it contains elevators for the use of the occupants. Residential buildings exceeding two stories must provide additional elevators if they are needed due to the proposed number of units or project design. Elevators shall be placed in locations which minimize the walking distance from the elevators to the residential units.
- 5) Perimeter fencing and for Planning Area 23b shall be provided in accordance with the Wall and Fence Plan depicted in **Exhibit IV-29, Wall and Fence**.
- 6) For Specific Plan Design Guidelines including general, landscape and architectural Design Guidelines, please refer to Section IV.C.
- 7) Garage setbacks in Planning Area 23b shall be a minimum of three feet (3') for interior streets and drives. All other building setbacks for interior streets and drives shall be a minimum of five feet (5'). Building setbacks from exterior boundary lines shall be five feet (5').
- 8) The range of allowable density permitted within the HHDR land use designation is 20 to 40 du/ac.

C. DESIGN GUIDELINES

In addition to the following Design Guidelines, all implementing development projects shall comply with the applicable provisions of the "Design and Landscape Guidelines for Development in the Second Supervisorial District" in effect at the time of implementing development approval.

1. General

The following Design Guidelines provide general direction for implementing project design at a land planning level, relating primarily to lot and street layouts and unit siting. These guidelines were developed in accordance with provisions of the Eastvale Area Plan and the design objectives of the *City of Eastvale I-15 Corridor Specific Plan*.

- a. Avoid long linear vistas and building edges within the development envelope and along the streetscape through variations in setbacks.
- b. Random setbacks of buildings and landscaping should be incorporated in all designs.
- c. Residential development shall be designed with varied setbacks and provide a mix of one and two-story type housing within the implementing project.
- d. All exterior materials and colors should be integrated through each development site to achieve continuity of design.
- e. Buildings should be designed to an appropriate human scale and should not to appear to be monumental or monotonous. The use of the following design elements will help in creating buildings properly scaled to people:
 - 1) Breaking up building masses into smaller, staggered masses;
 - 2) Breaking up long wall surfaces and roof lines into discontinuous surfaces;
 - 3) Randomly textured materials on roofs and walls;
 - 4) Extended roof overhangs.
- f. The height and bulk of buildings should be appropriate to the size, shape and topography of the site and in harmony with its setting.
- g. Parking areas should be designed to facilitate both vehicular and pedestrian movements.
- h. The siting and design of structures within each planning area should consider the proper orientation to prevalent environmental conditions; sun, wind, terrain, views and vegetation.
- i. The siting and design of structures and landscaping should be sensitive to the modified terrain so as not to dominate the landform as seen from lower elevations.

2. Architectural Qualities and Concepts

The architectural character of the *City of Eastvale I-15 Corridor Specific Plan* will not focus on any particular architectural theme or style. Rather than restricting architectural style, these guidelines will define the character and quality of the community of implementing projects in the *City of Eastvale I-15 Corridor Specific Plan*. These guidelines provide a conceptual reference for architectural continuity and visual cohesiveness. As such, this section should not be interpreted to require a stringent compliance to any particular style of architecture.

The following architectural design guidelines provide direction for the exterior architecture of future residential and non-residential structures in the *City of Eastvale I-15 Corridor Specific Plan*. The following guidelines describe four characteristics of structural architecture, as well as recommendations for signage, lighting and equipment screening. These guidelines are not intended to be all-inclusive and variations from specified elements or materials are permitted. Listed features and elements are considered appropriate or acceptable but not necessarily required.

a. Sensitivity

The architectural styles and treatments selected for implementing projects within the *City of Eastvale I-15 Corridor Specific Plan* should exhibit the following characteristics of sensitivity:

- Creates a complementary relationship with adjacent projects;
- Creates architectural continuity for projects within the *City of Eastvale I-15 Corridor Specific Plan*;
- Authentically replicates selected styles;
- Implementing projects structural integrity;
- Develops a compatible relationship between implementing projects/buildings and open space or recreation areas;
- Presents an appropriate orientation toward adjacent land uses; and
- Affect an aesthetically pleasant profile for the I-15 Corridor.

b. Effect

The architectural styles and treatments selected for implementing projects within the *City of Eastvale I-15 Corridor Specific Plan* should create the following effects:

- Establishes and enhances overall character;
- Emphasizes proper land use relationships;
- Avoids visual repetition;
- Creates a desirable visual environment;
- Authentic renditions of selected styles;

- Creates vitality through interaction of styles; and
- Maintains continuity through the use of similar architectural elements.

c. Features

The architectural styles and treatments selected for implementing projects within the *City of Eastvale I-15 Corridor Specific Plan* should utilize the following features:

- Articulated facades
- Variegated color palettes with coordinated:
- Low plate lines
- Hip roofs
- Large overhangs
- Changing roof plans
- Horizontal elements
- Recessed entries
- Side lit doors
- Greenhouses
- Shutters
- Awnings
- Columns
- Balconies
- Broad porches
- Greenhouses
- Extensive windows
- Bay/oriel windows
- Paned doors

d. Materials

- Stucco and plaster
- Wood and dimensioned timber
- Board on board
- Stone or rock
- Brick
- Wood shingles

- Roof tile
- Slate
- Metal-seamed roof panels
- Fascia
- Metal or wood window dividers
- Metal or wood railings
- Clear/beveled/etched/frosted/tinted or reflective glass
- Glass block
- Pre-cast concrete
- Sandblasted concrete
- Split-faced block

e. Signage

- 1) The design of identification, for sale, lease or rent and directional signs including the location, materials, colors, copy and the method of signing, size, and construction shall be approved by the City in accordance with the existing Sign Ordinance, except as noted herein.
- 2) Identification signs are restricted to advertising only the person or company located on the lot. Moving or flashing signs are prohibited. Internally lit signs are preferred.
- 3) All ground signs shall not be located closer than five feet (5') to any property line.
- 4) All monument signs shall not exceed a height of ten feet (10') measured vertically from the base at ground level to the apex of the sign.
- 5) The area of each directional sign may not exceed four square feet. Maximum height shall be four feet (4'). The sign shall be used for directional purposes only.
- 6) Signs should be used for the purpose of identification and direction. The design of permitted signs should be architecturally integrated with the building design.
- 7) Signs shall be located outside of sight triangles per City of Eastvale standards to avoid sight distance issues at all public and private intersections.

f. Lighting

- 1) The design of light fixtures and their structural support shall be architecturally compatible with the surrounding buildings.
- 2) Light standards shall not exceed twenty feet (20') in height and in no case shall they exceed the height of the buildings on site.

- 3) All parking lot and driveway lighting should provide uniform illumination. Accent illumination is recommended at key points such as entrances, exits, loading zones, and drives.
- 4) Lighting should be shielded and situated so as to not cause glare or excessive light spillage on neighboring sites.

g. Equipment Screening (commercial uses only)

- 1) All roof and ground-mounted equipment shall be screened from public view on all sides.
- 2) All screening shall be architecturally integrated with the building design and a roof parapet wall shall be used to screen roof-mounted equipment. Roof equipment screening where building overviews occur from adjacent streets and the freeway shall incorporate visual enclosure of equipment.

3. Landscape Components

The landscape design guidelines detailed in this section establish a reference for the planting of public rights-of-way and common areas. The guidelines include a plant palette and illustrative plans which reflect the quality and image of the *City of Eastvale I-15 Corridor Specific Plan*. The guidelines recognize and encourage the use of landscaping in complementing and enhancing the implementing project architecture as shown in **Exhibit IV-9, Conceptual Landscape Plan**.

a. Design Concept

The landscape design concept creates a community theme that reinforces several functional goals such as traffic, circulation and land use definition through the incorporation of historical themes and local character. The plantings are decorative while fulfilling the functional needs that each plant material provides. Plant materials chosen in the following lists have been selected for their suitability to the area (i.e., they will tolerate a wide temperature range, have low water consumption requirements, withstand local wind conditions and smog, and additionally, have an inherent shape that appear maintained without assistance. The plants listed within the following groupings have a natural form that matches the forms shown within the accompanying sections, and are drought tolerant.

The landscape design established within this development is based upon the idea of simplicity. The diversity in the selection of plant material is great enough to provide variation and safety from landscape failure caused by disease within a monoculture, yet simple enough to create a design statement which is easy and cost-effective to maintain on a long-term basis.

Although the character of Limonite Avenue could be differentiated from Hamner Avenue in that they border different land uses, respectively, they are the major north/south and east/west rights-of-way transecting this Specific Plan. Therefore, the general theme character that binds

the varying land uses within the development has been established along these streets and for continuity sake, the character of smaller hierarchy rights-of-way are created from these primary design elements.

It must further be noted that most streets aligned in an east/west direction have been provided with bi-level planting comprised of a high-branching Eucalyptus windrow and lower growing shrub planting and/or a low growing tree. This street profile has been modified for each street with the east/west orientation. This treatment is necessary to assist in the abatement of a strong northerly prevailing wind. If any trees are determined to result in sight distance issues, they shall be removed and/or replaced by the Planning Director.

b. Streetscape

It is the intent of these landscape design guidelines to establish a recognizable identity for the *City of Eastvale I-15 Corridor Specific Plan*. The primary landscape element of this Specific Plan is the streetscape utilized on the major streets. Three separate streetscapes have been developed for the *City of Eastvale I-15 Corridor Specific Plan*. Each streetscape incorporates its own plant palette and design scheme. The plant palettes are rather broad to allow for flexibility in isolated planting schemes while unifying the implementing projects through a cohesive landscape theme.

The materials on the plant lists have been specifically chosen for the adaptability soil and climatic conditions of the area, suitability for various landscape goals and relatively low maintenance characteristics.

1) Streetscape 1/Limonite Avenue

Commercial development is located along both sides of Limonite Avenue as it transects this Specific Plan. Although rural in character, the landscape design provided for this corridor is more formal in nature relative to the design character of other streetscapes within this Specific Plan.

The planting is designed to provide an identity for the commercial corridor while maintaining a low profile and/or a "view window" through the plant material at the eye level to retain visibility to commercial enterprises.

To accomplish these goals, an interrupted, high-branching Eucalyptus windrow is established as a backdrop to a forefront of a formal lineal arrangement of Flowering Plum trees set in lawn. Between these two rows of trees is a low formal, dark green hedge to visually screen the parking lot of the commercial area.

In areas of interrupted windrow, the lawn area is replaced by a low, grey-green, arc-shaped shrub hedge border within which is planted a rose-colored, low groundcover.

The location of Streetscape 1 is shown on **Exhibit IV-9, Conceptual Landscape Plan**. Typical section and plan views of Streetscape 1 are shown on **Exhibit IV-15, Streetscape 1 Section** and **Exhibit IV-16, Streetscape 1 Plan**. The following plant materials have been selected to create this character for Streetscape 1:

a) Trees

Eucalyptus camaldulensis

Red Gum

Prunus cerasifera "Krauter Vesuvius"

Flowering Plum

b) Shrubs

Juniperus squamata "Blue Star"

NCN (no common name)

Viburnum suspensum

Sandankwa Viburnum

c) Groundcovers

Hypericum calycium

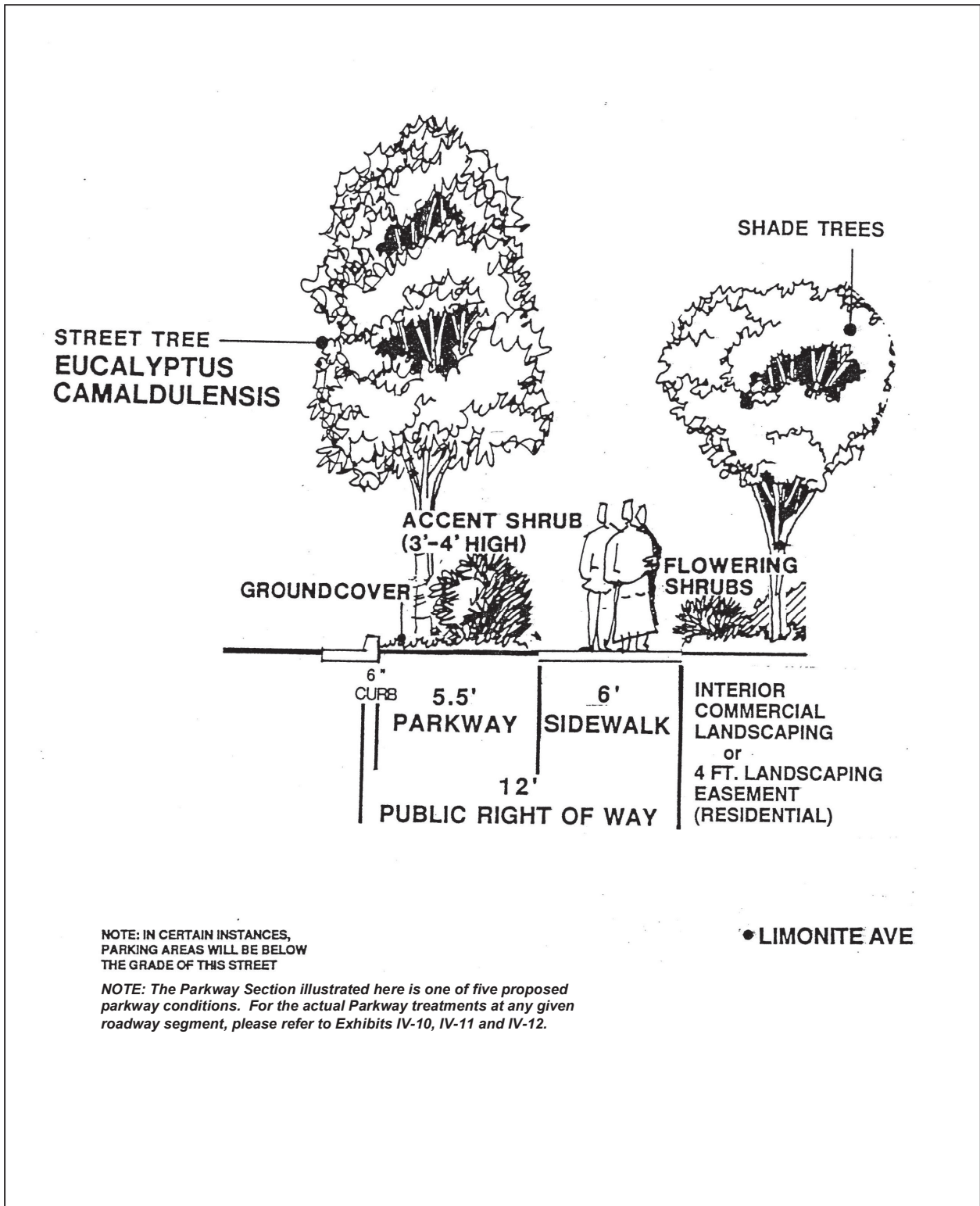
Aaron's Beard

Lawn - *Alta Fescue*

Tall Fescue

Polygonum capitatum

Pink Clover Blossom



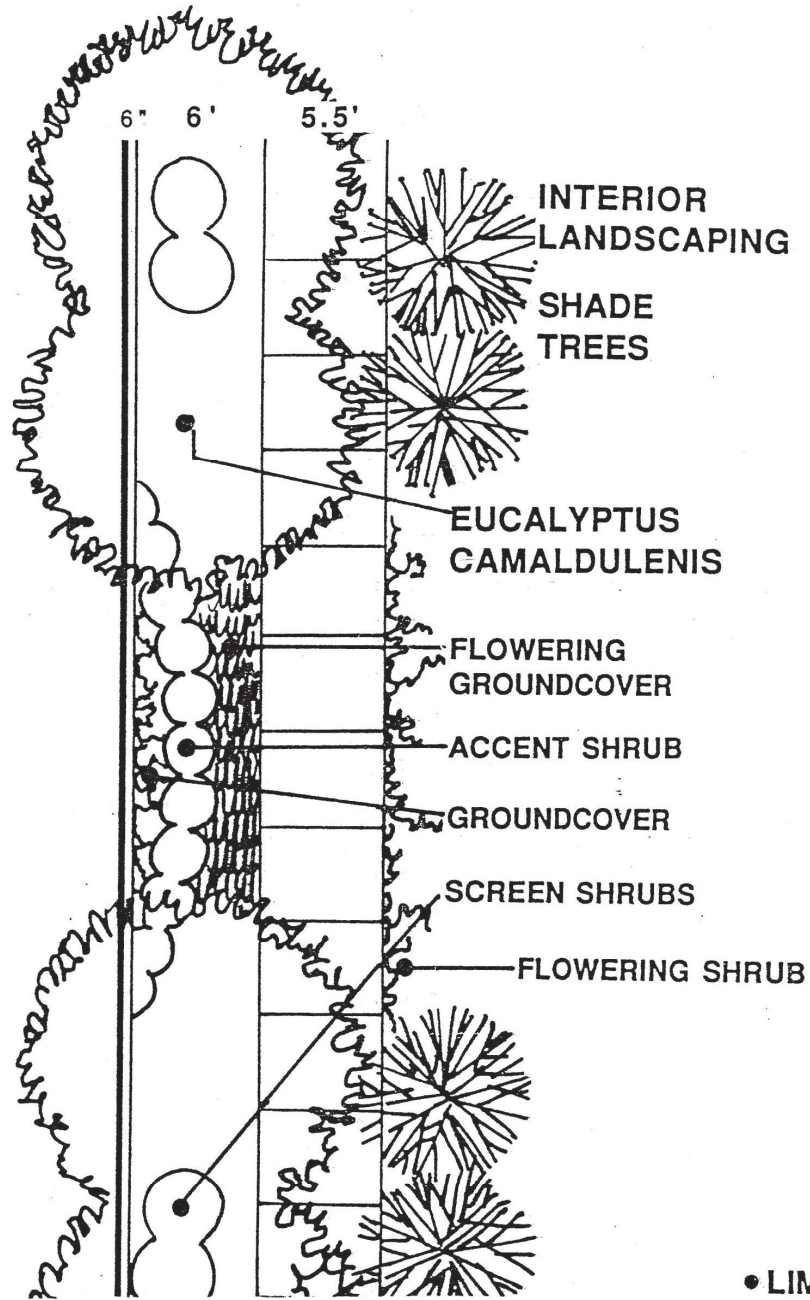
NOTE: IN CERTAIN INSTANCES, PARKING AREAS WILL BE BELOW THE GRADE OF THIS STREET

NOTE: The Parkway Section illustrated here is one of five proposed parkway conditions. For the actual Parkway treatments at any given roadway segment, please refer to Exhibits IV-10, IV-11 and IV-12.

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Exhibit IV-15
STREETSCAPE 1 SECTION



NOTE: The Parkway Section illustrated here is one of five proposed parkway conditions. For the actual Parkway treatments at any given roadway segment, please refer to Exhibits IV-10, IV-11 and IV-12.

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Exhibit IV-16 STREETSCAPE 1 PLAN

2) Streetscape 3/Hamner Avenue

The location of Streetscape 3 is shown on **Exhibit IV-9, Conceptual Landscape Plan**. Typical section and plan views of Streetscape 3 are shown on **Exhibit IV-17, Streetscape 3 Section** and **Exhibit IV-18, Streetscape 3 Plan**. The following plant palette has been established for Streetscape 3:

a) Tree

Jacaranda acutifolia

Jacaranda

b) Shrubs

Hebe "Rubra"

NCN

Hemerocallis hybrids

Daylily

Xylosma congestum "Compacta"

Dwarf Shiny Xylosma

c) Groundcovers

Hypericum calycinum

Aaron's Beard

Lawn - Alta fescue

Tall Fescue

Lonicera japonicum "Halliana"

Hall's Honeysuckle

Polygonum capitatum

Pink Clover Blossom

Santolina species

Lavender Cotton

Teucrium chamaedrys "Prostratum"

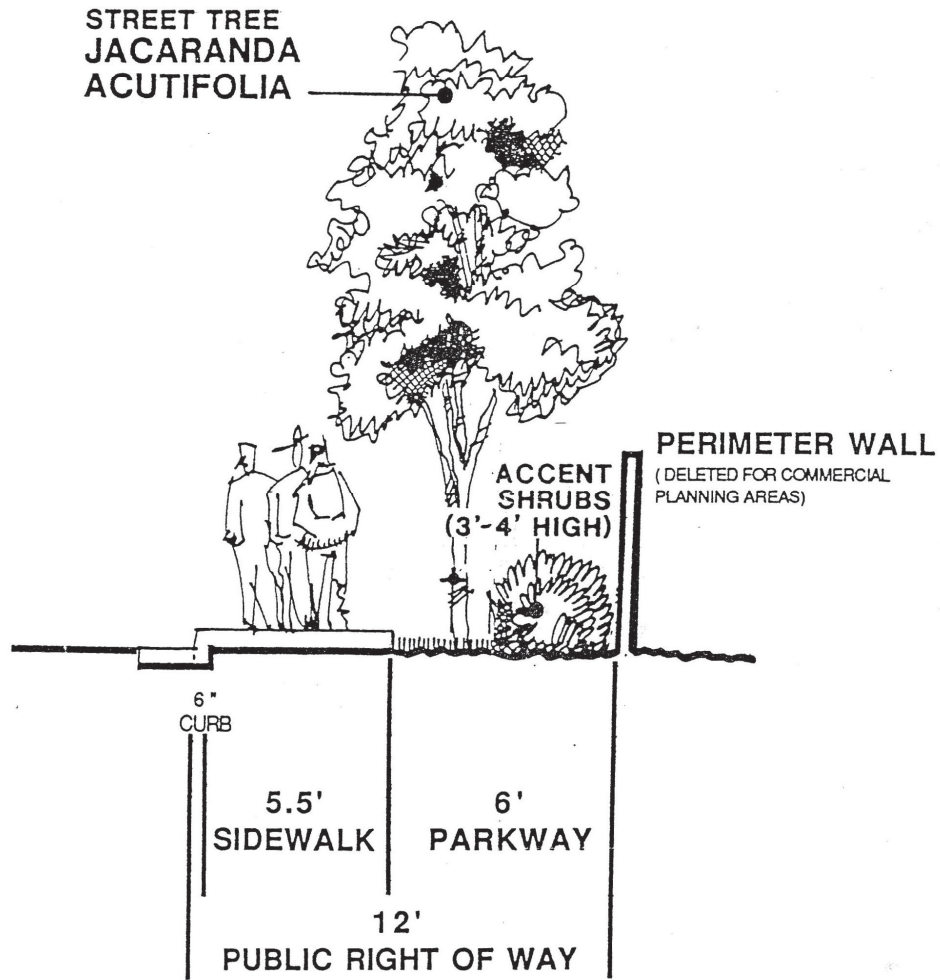
Dwarf Germander

Trachelospermum jasminoides

Star Jasmine

Verbena peruviana

NCN



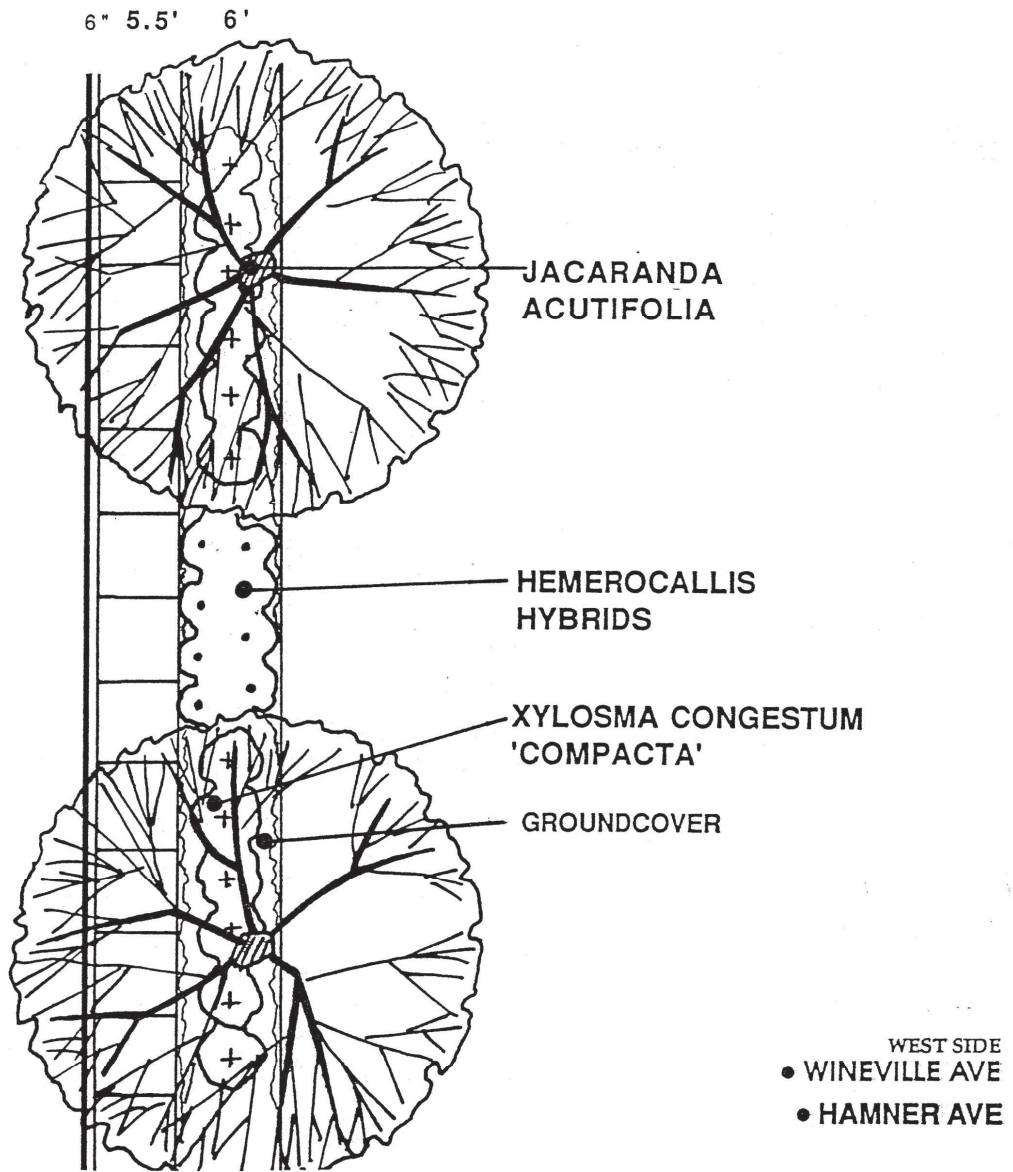
NOTE: The Parkway Section illustrated here is one of five proposed parkway conditions. For the actual Parkway treatments at any given roadway segment, please refer to Exhibits IV-10, IV-11 and IV-12.

- WEST SIDE
- WINEVILLE AVE
- HAMNER AVE

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Exhibit IV-17 STREETSCAPE 3 SECTION



NOTE: The Parkway Section illustrated here is one of five proposed parkway conditions. For the actual Parkway treatments at any given roadway segment, please refer to Exhibits IV-10, IV-11 and IV-12.

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Exhibit IV-18 STREETSCAPE 3 PLAN

3) Streetscape 5/Bellegrave Avenue

The design character for Bellegrave Avenue is the most rural of the profiles for this Specific Plan, due to adjacent land uses to retain the concept of their existing character. This provides a natural visual transition while retaining historical context for the area.

The location of Streetscape 5 is shown on **Exhibit IV-9, Conceptual Landscape Plan**. Typical section and plan views of Streetscape 5 are shown on **Exhibit IV-19, Streetscape 5 Section** and **Exhibit IV-20, Streetscape 5 Plan**. The following planting palette has been established for Streetscape 5:

a) Tree

Eucalyptus camaldulensis Red Gum

b) Shrubs

Cassia armata NCN

Hebe "Coed" NCN

Hebe "Rubra" NCN

Hemerocallis hybrids Daylily

Juniperus sabina "Tamarisifolia" Tam Juniper

Lantana camara Lantana

Leptospermum scoparium "Nanum" Dwarf Manuka

Nerium oleander "Petit Salmon" Dwarf Oleander

Pittosporum tobira "Wheeleri" Wheeler's Dwarf

Pittosporum tobira "Variegata" Variegated Mock Orange

Teucrium fruticans Bush Germander

Xylosma congestum "Compacta" Dwarf Shiny Xylosma

c) Groundcovers

Hypericum calycinum Aaron's Beard

Juniperus sabina "Tamarisifolia" Tam Juniper

Lawn - *Alta fescue* Tall Fescue

Lonicera japonica "Halliana" Hall's Honeysuckle

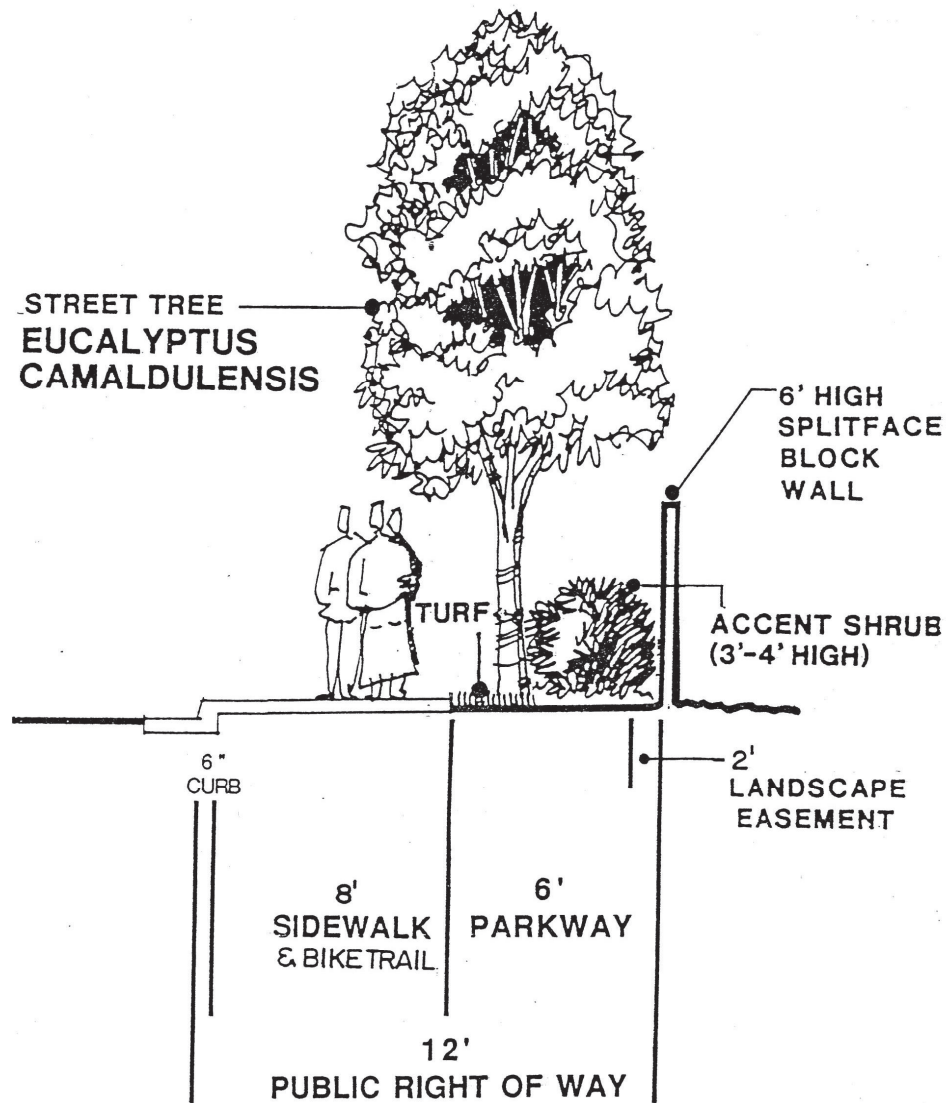
Polygonum capitatum Pink Clover Blossom

Rosmarinus officinalis Rosemary

Santolina species Lavender Cotton

Trachelospermum jasminoides Star Jasmine

Vinca major Periwinkle



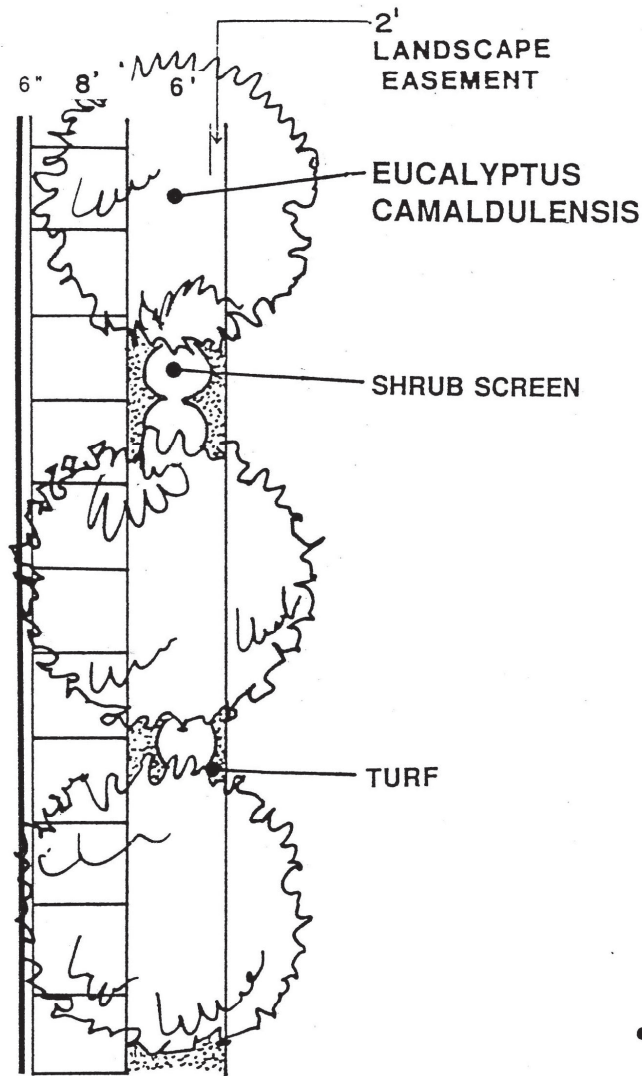
NOTE: The Parkway Section illustrated here is one of five proposed parkway conditions. For the actual Parkway treatments at any given roadway segment, please refer to Exhibits IV-10, IV-11 and IV-12.

● BELLEGRAVE AVE

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Exhibit IV-19 STREETSCAPE 5 SECTION



NOTE: The Parkway Section illustrated here is one of five proposed parkway conditions. For the actual Parkway treatments at any given roadway segment, please refer to Exhibits IV-10, IV-11 and IV-12.

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Exhibit IV-20 STREETSCAPE 5 PLAN

c. Entries and Intersections

The major determinate element of character for the two-tier hierarchy of entries and intersections is the California Pepper tree. This tree has been specified as the unifying element that defines these junctions. The design layout for major entries and intersections is essentially the same, as is the design layout for secondary entries and intersections, with a few minor deviations. Also, the design scheme layout between the two hierarchy categories, major and secondary, is comprised of the same elements, the difference being in scale and elaboration. The location of all major and secondary entries and intersections is shown on **Exhibit IV-9, Conceptual Landscape Plan**. Typical plans and elevations of entry and intersection treatments are illustrated in **Exhibit IV-21** through **Exhibit IV-26**.

The major entries have a group of vertical evergreen trees that provide a backdrop to the multi-trunk, broad-dome California Pepper. This backdrop of trees is not included in the secondary entries. All schemes have landscape mounding, as well as mid-height background shrubs, and a forefront of lawn. All schemes, except the secondary intersections have a focal point of a colored groundcover mass.

The variation between intersections and entries for all hierarchy levels is the inclusion or exclusion of a wall monument for Specific Plan signage. Entries are essentially intersections with a wall monument. The major entry monument is greater in scale and more complex than the secondary entry, but all other design elements are essentially the same.

The following plant palette has been established for entries and intersections:

1) Trees

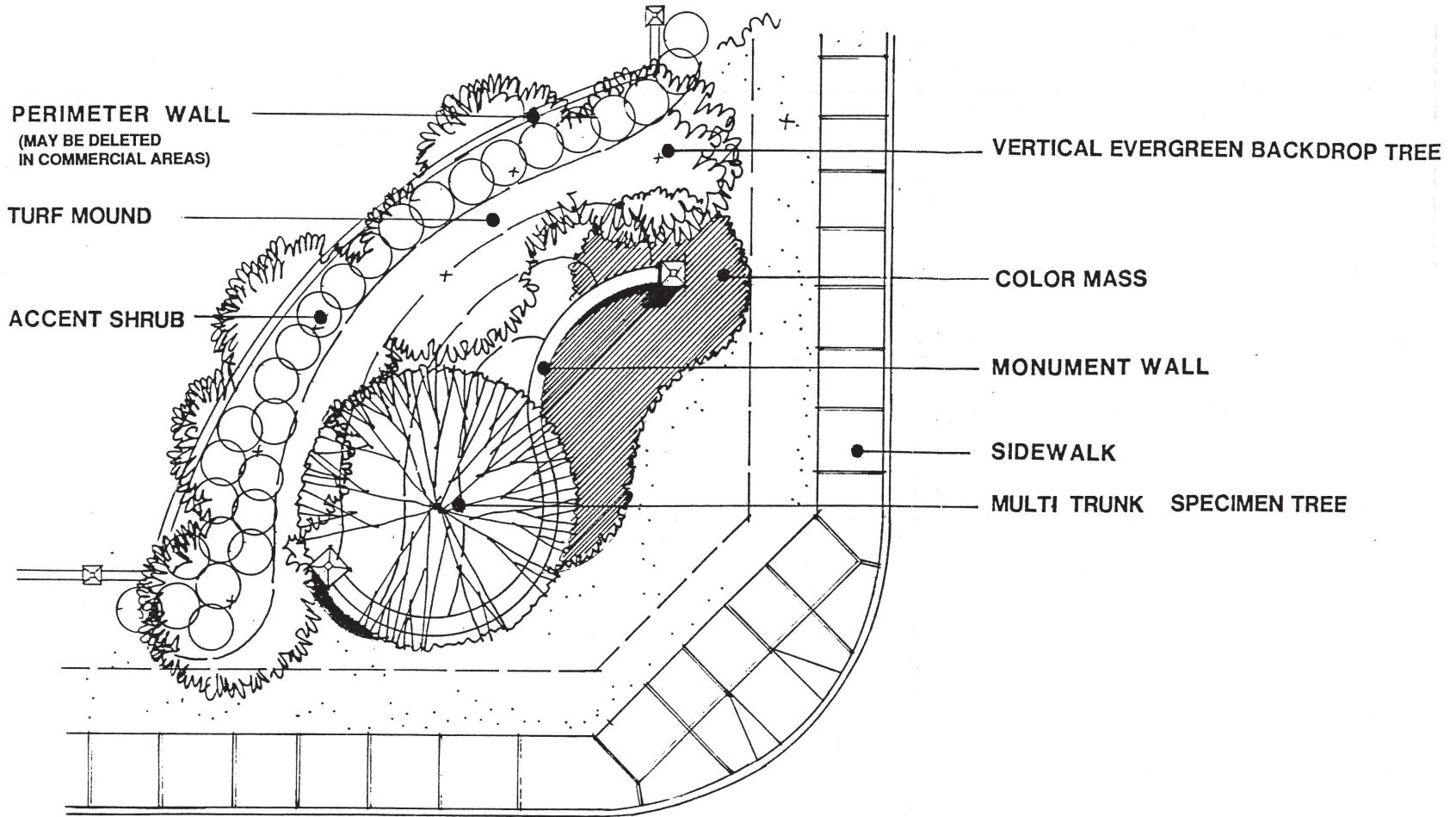
<i>Pinus halepensis</i> (Backdrop tree)	Aleppo Pine
<i>Schinus molle</i>	California Pepper

2) Shrubs

<i>Grevillea "Noellii"</i>	NCN
<i>Leptospermum scoparium</i>	Dwarf Manuka
<i>Leucophyllum fruticans</i>	Texas Ranger
<i>Nerium oleander</i>	Oleander
<i>Pittosporum tobira "Variegata"</i>	Variegated Mock Orange
<i>Raphiolepis indica</i>	India Hawthorne
<i>Viburnum suspensum</i>	Sandankwa Viburnum

3) Groundcovers

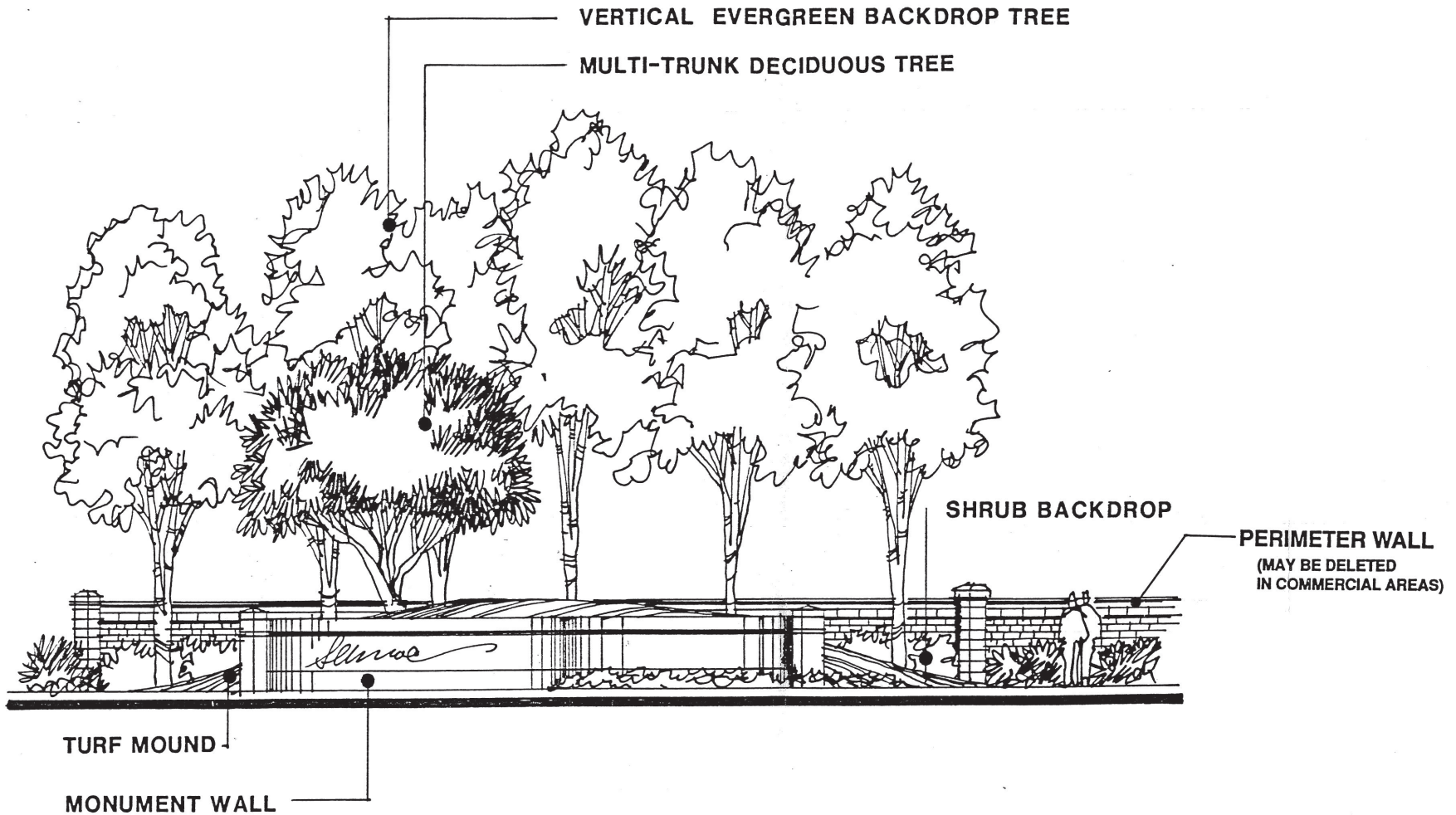
<i>Dimorphoteca sinuata</i>	Cape Marigold
"Marathon" <i>Alta fescue</i>	Tall Fescue



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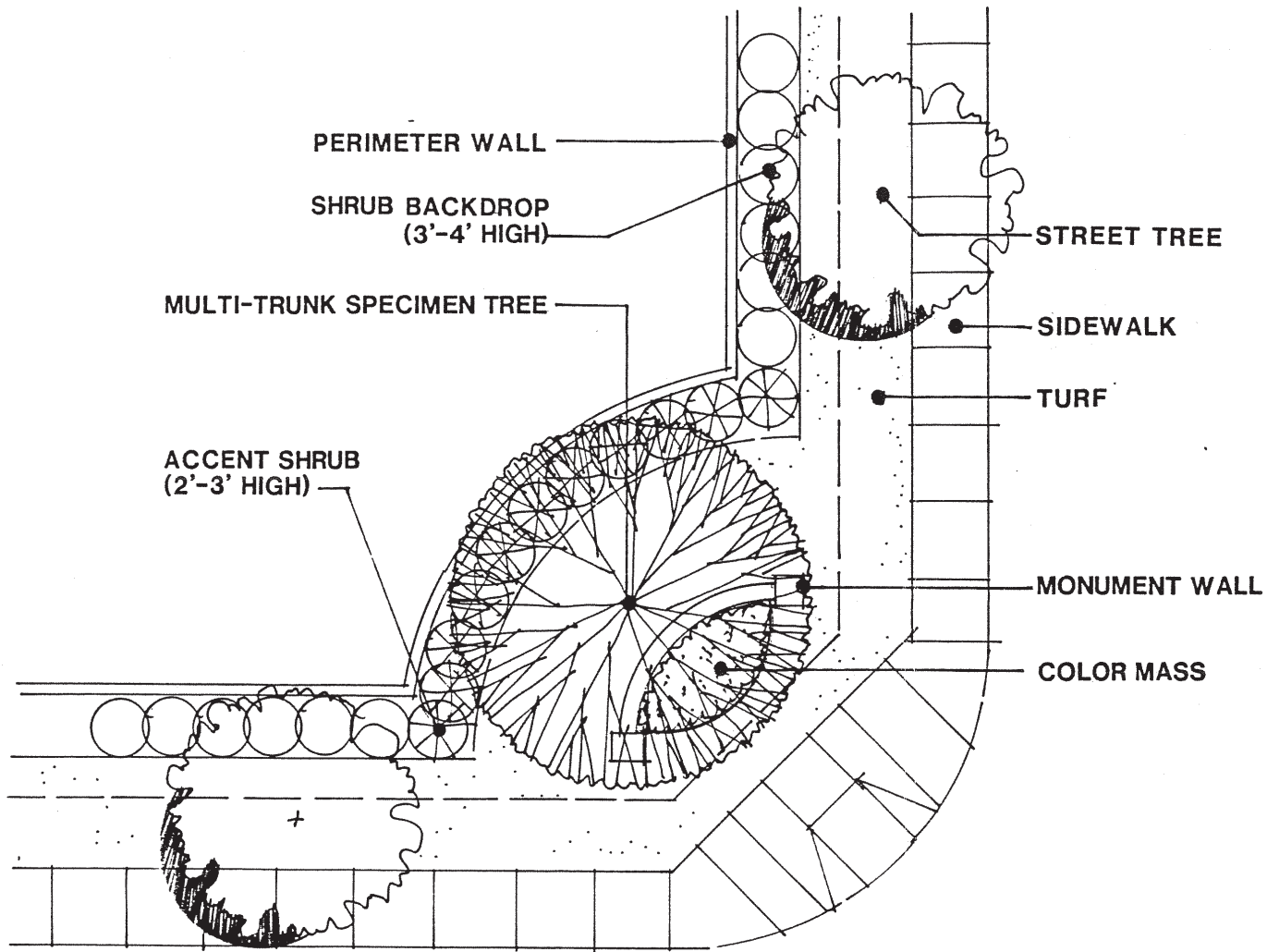
Exhibit IV-21
MAJOR ENTRY PLAN



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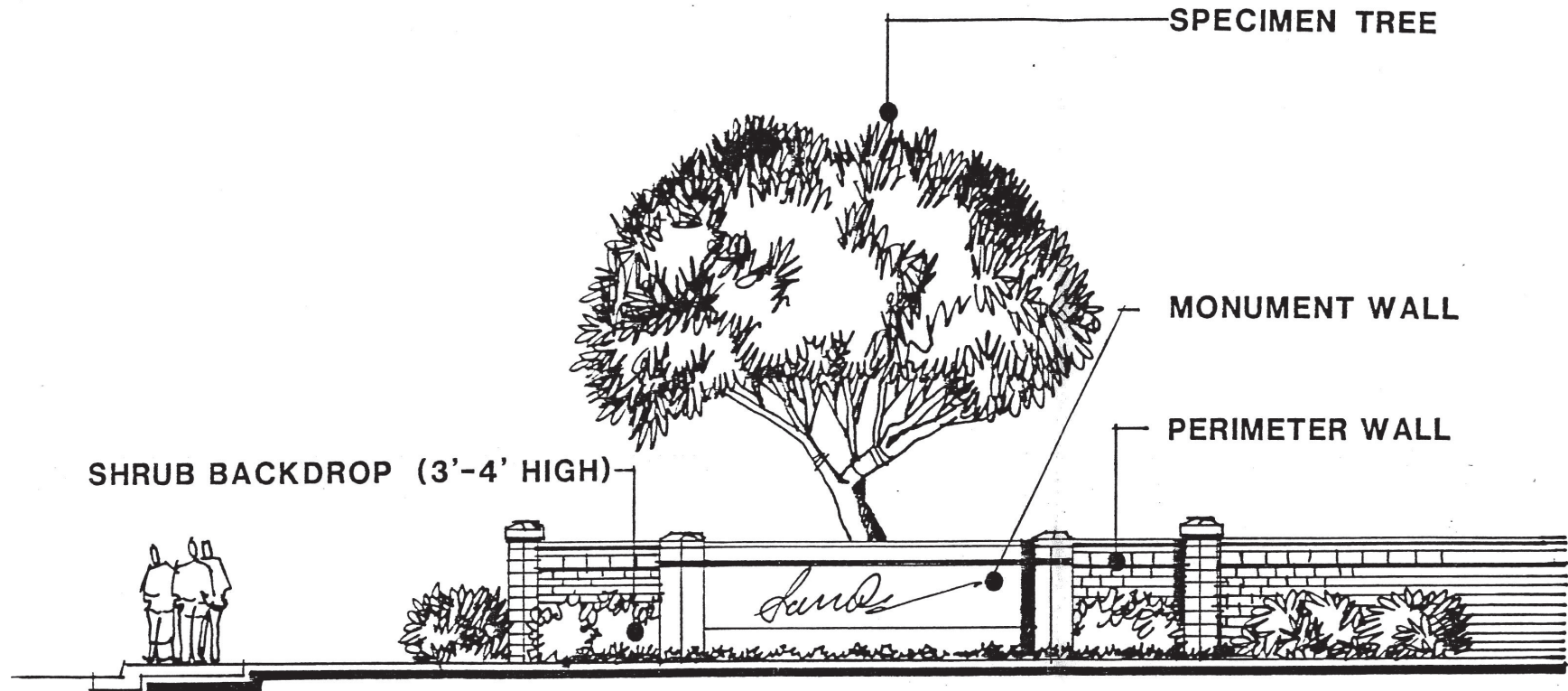
Exhibit IV-22
MAJOR ENTRY ELEVATION



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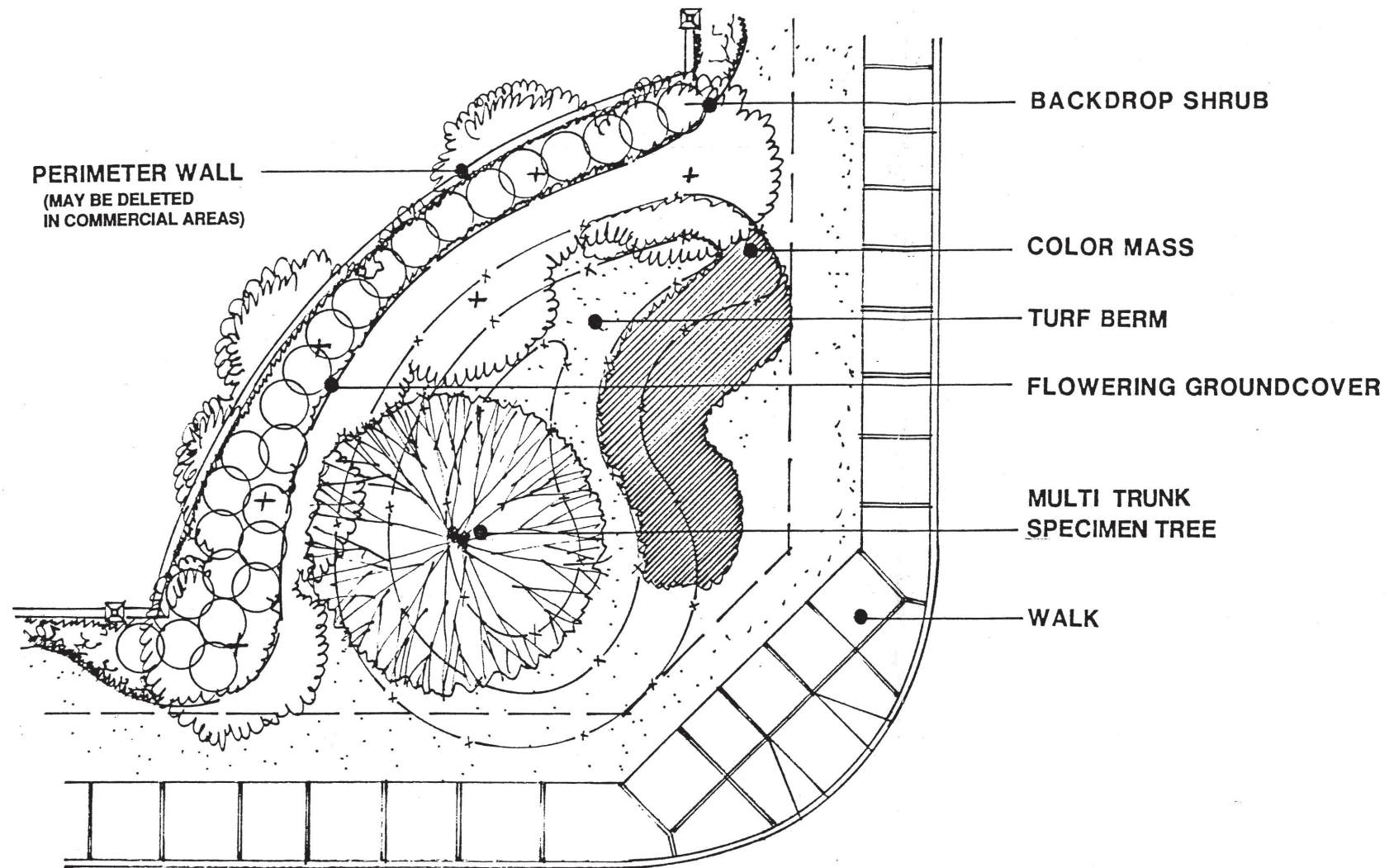
Exhibit IV-23
SECONDARY ENTRY PLAN



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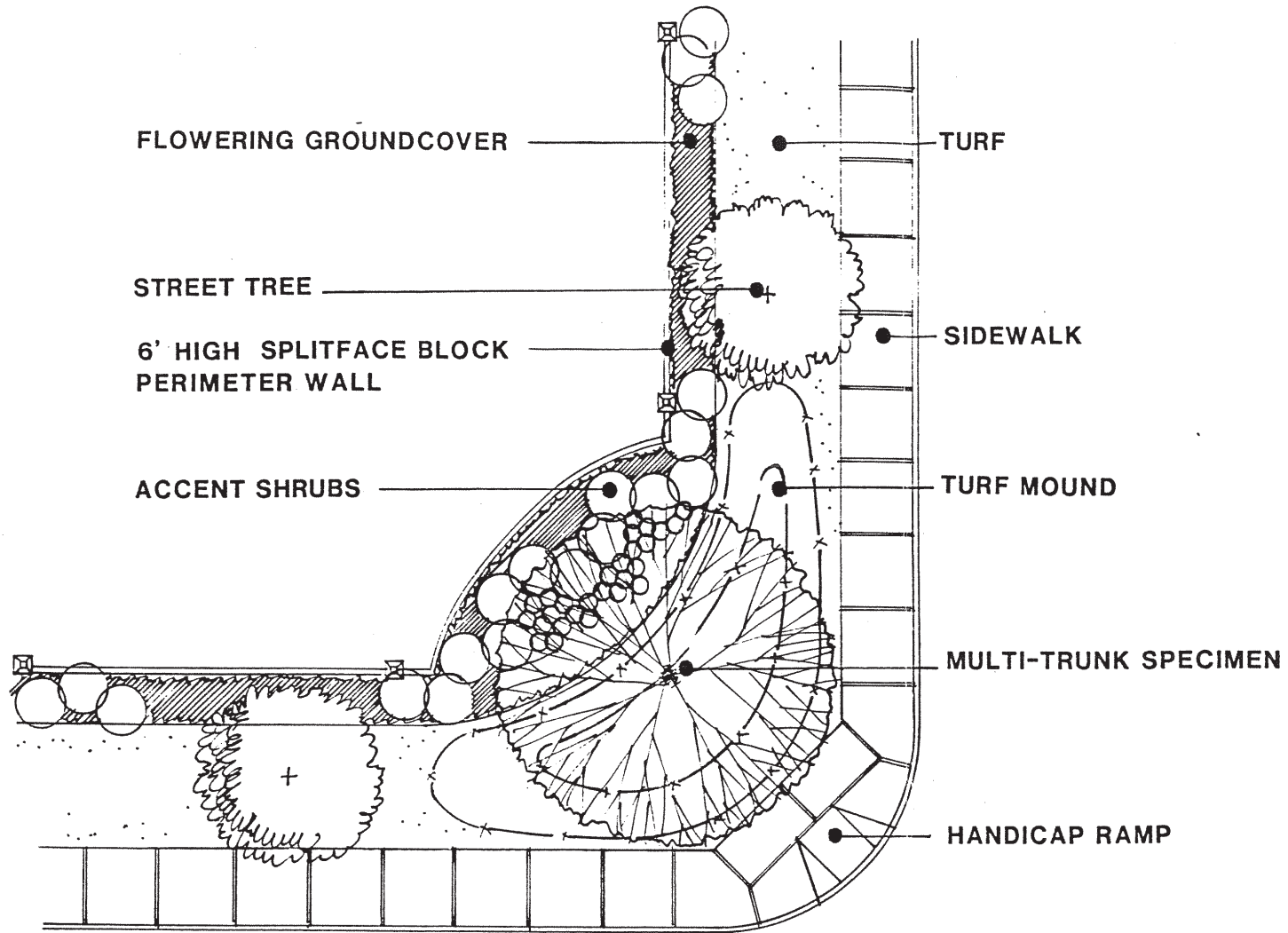
Exhibit IV-24
SECONDARY ENTRY ELEVATION



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Exhibit IV-25
MAJOR INTERSECTION PLAN



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Exhibit IV-26
SECONDARY INTERSECTION PLAN

d. Buffer Treatments and Freeway Enhancement Zones

The character of the Landscape Buffer Zone is based upon the Eucalyptus windrow element defined within some of the streetscapes. Landscape buffers function to visually screen two adjacent land uses from each other. These buffers are located internally or as an edge treatment, as shown on the plan on **Exhibit IV-9, Conceptual Landscape Plan**. Freeway Enhancement Zones are located solely as an edge treatment between the freeway right-of-way and commercial land uses. The Freeway Enhancement Zone has been designed to not have total capacity to screen out views, in that, it is mandatory to maintain visual access from the freeway into the commercial uses which it borders. Therefore, on a functional level, the requirements of the landscape layout are similar to the Streetscape 3 design, which provides a "view window" into the commercial areas. Therefore, this Specific Plan and section shall also prevail for the Freeway Enhancement Zone, but the plant palette shall be different to define a change in the character. Typical section and plan views of the landscape buffer and freeway enhancement treatments are shown on **Exhibit IV-27, Buffer Treatment Section** and **Exhibit IV-28, Buffer Treatment Plan**. The following plant palette has been established for both the Landscape Buffer and Freeway Enhancement Zones:

1) Trees

<i>Acacia melanoxylon</i>	Blackwood Acacia
<i>Cassia leptophylla</i>	Gold Medallion Tree
<i>Cinnamomum camphora</i>	Camphor Tree
<i>Cupressocyparis leylandii</i>	NCN
<i>Eucalyptus sideroxylon</i>	Rosy-Red Ironbark
<i>Pinus halepensis</i>	Aleppo Pine
<i>Podocarpus gracilior</i>	Fern Pine

2) Shrubs

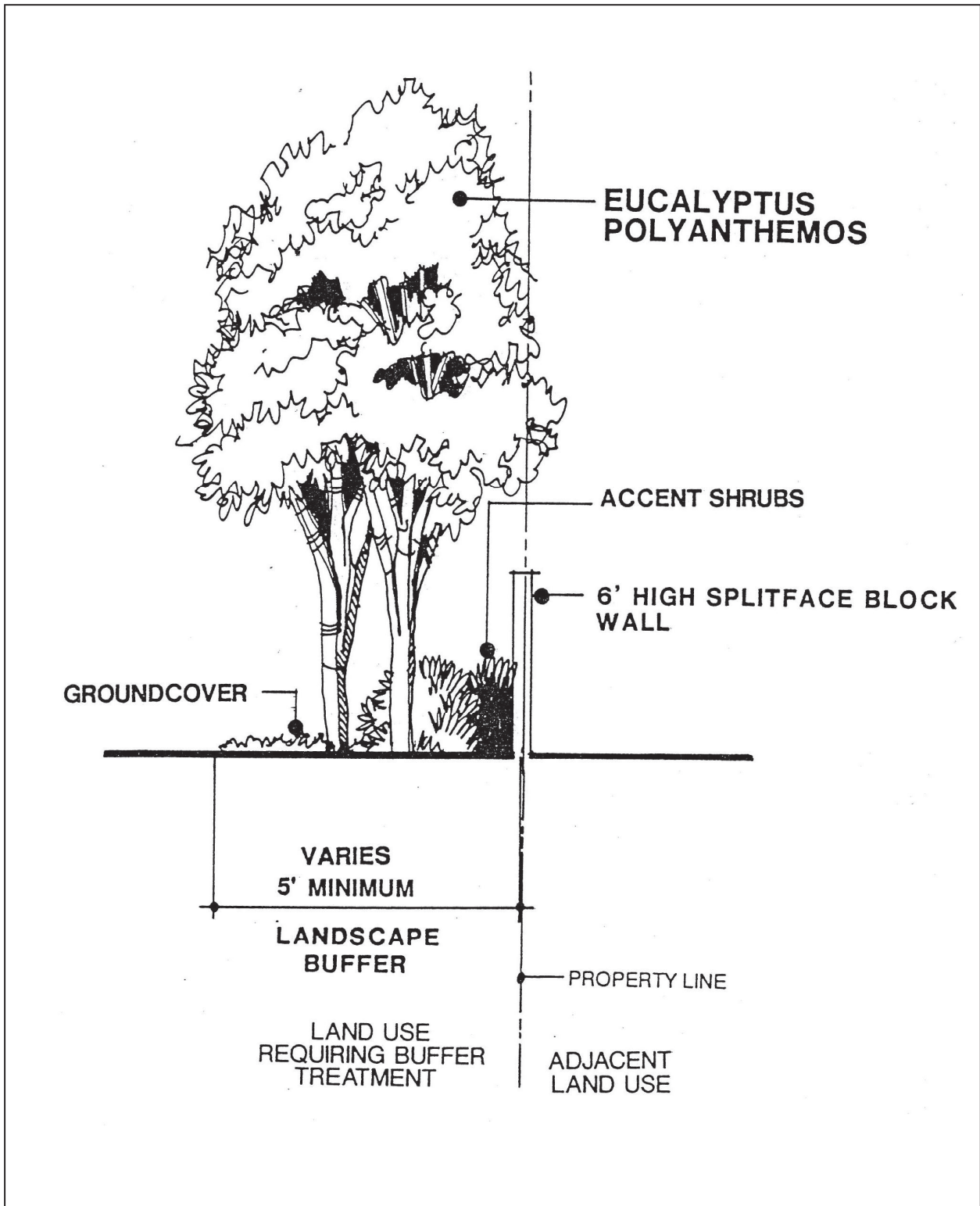
<i>Grevillea "Noellii"</i>	NCN
<i>Juniperus chinensis "Armstrongii"</i>	Armstrong Juniper
<i>Nerium oleander</i>	Oleander
<i>Photinia "Fraseri"</i>	NCN
<i>Pittosporum tobira "Variegata"</i>	Variegated Mock Orange
<i>Raphiolepis indica</i>	India Hawthorne
<i>Viburnum suspensum</i>	Sandankwa Viburnum

3) Groundcovers

<i>Berberis repens</i>	Creeping Barberry
<i>Hypericum calycinum</i>	Aaron's Beard
<i>Lonicera japonica "Halliana"</i>	Hall's Honeysuckle

Polygonum capitatum
Rosmarinus officianalis
Santolina species
Trachelospermum jasminoides

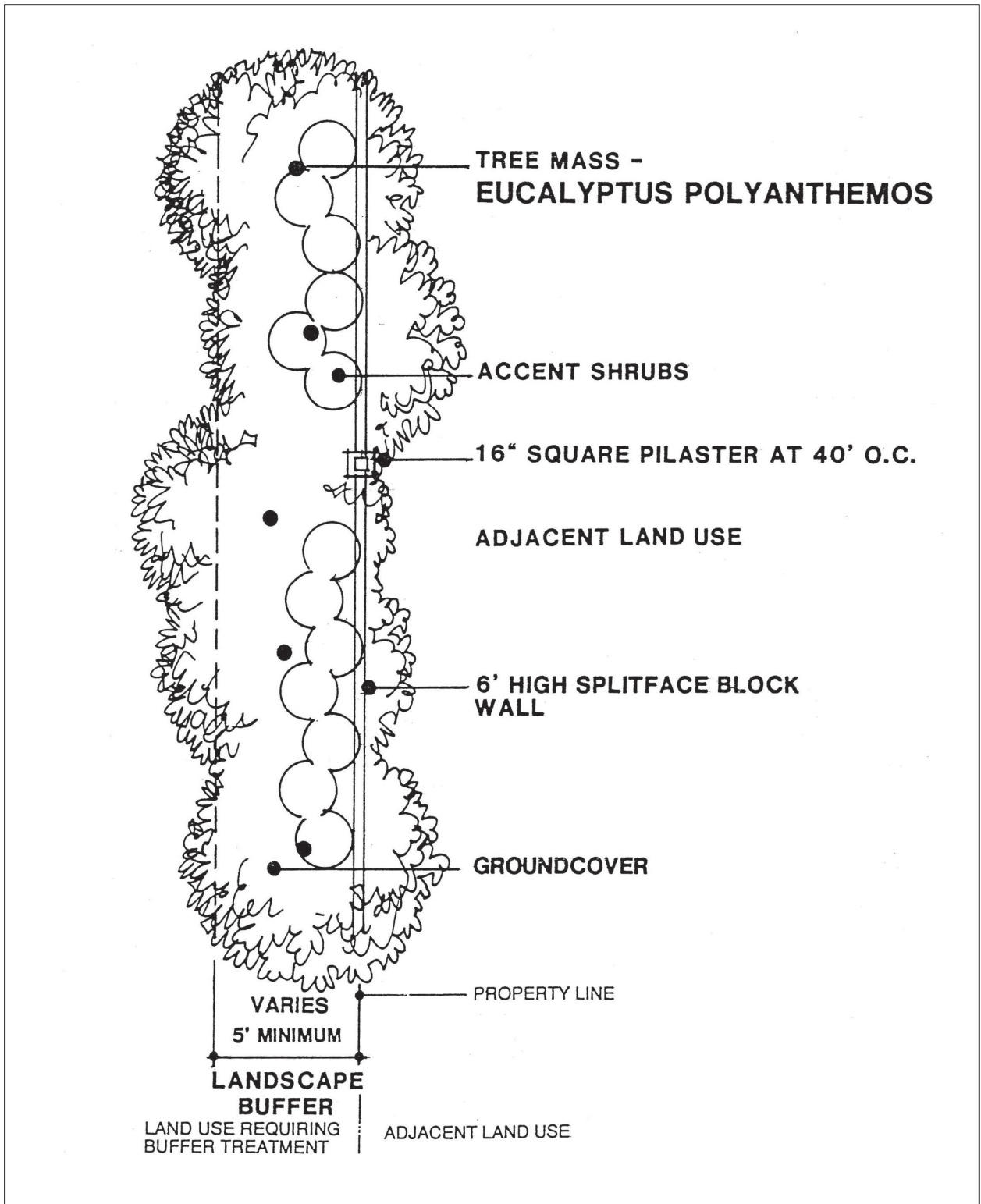
Pink Clover Blossom
Rosemary
Lavender Cotton
Star Jasmine



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Exhibit IV-27 BUFFER TREATMENT SECTION



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Exhibit IV-28 BUFFER TREATMENT PLAN

e. Landscape Guidelines and Requirements

1) Landscape Guidelines

- a) Parking areas shall incorporate both landscaping and screening to make them visually compatible with their surroundings.
- b) Grouped masses of plant material shall be designed to complement architectural elevations and rooflines through color, texture, density, and form on both the vertical and horizontal planes.
- c) The preservation of existing mature trees and their integration into introduced landscape materials shall be required where feasible.
- d) Landscaping shall be designed to establish implementing project identity and to accentuate common entrance areas. Landscaping and berms shall be used to screen parking areas and non-residential storage areas.
- e) Wherever feasible, native and drought-tolerant plant material shall be used and existing mature trees preserved.
- f) Appropriate plant materials shall be used to define space, create a visual image and separate differing land uses.
- g) Landscaping shall consider solar rights of adjacent structures.

2) Landscaping Requirements

- a) Residential development shall include front yard landscaping.
- b) All front and side setback areas, and rear setbacks where abutting non-industrial uses shall be effectively landscaped with compositions of landscape elements to provide visual screening and achieve a transition into the primary use area of the site. These landscape elements include earth berming, groundcover, shrubs and trees.
- c) Utility services and enclosures shall be screened from views from streets and adjacent properties with landscape materials and barrier treatments.
- d) In order to achieve a uniform landscape theme within this Specific Plan, the areas within the street medians, park strips, and streetscape setback areas shall have an established landscape materials pallet consistent with the Specific Plan.
- e) A permanent automatic underground sprinkler system shall be installed where appropriate which shall be capable of providing the proper amount of precipitation for the particular type of plant materials used.
- f) Graded but undeveloped areas for future development will be maintained in a weed-free condition.
- g) Landscaping, in accordance with the approved landscape plans shall be installed prior to occupancy of structures. If seasonal conditions do not permit planting, interim erosion control measures shall be approved by the City.

4. Walls and Fences

The network of walls and fences for the *City of Eastvale I-15 Corridor Specific Plan* serves to promote the identity of this Specific Plan while separating individual or incompatible uses. Walls and fences, like the plant materials, provide design character and function to visually unify the divergent land uses into one definable project. A hierarchy of wall and fencing types has been developed for various purposes as follows:

- Perimeter Wall
- Theme Wall
- Perimeter Fence
- Equestrian Fence

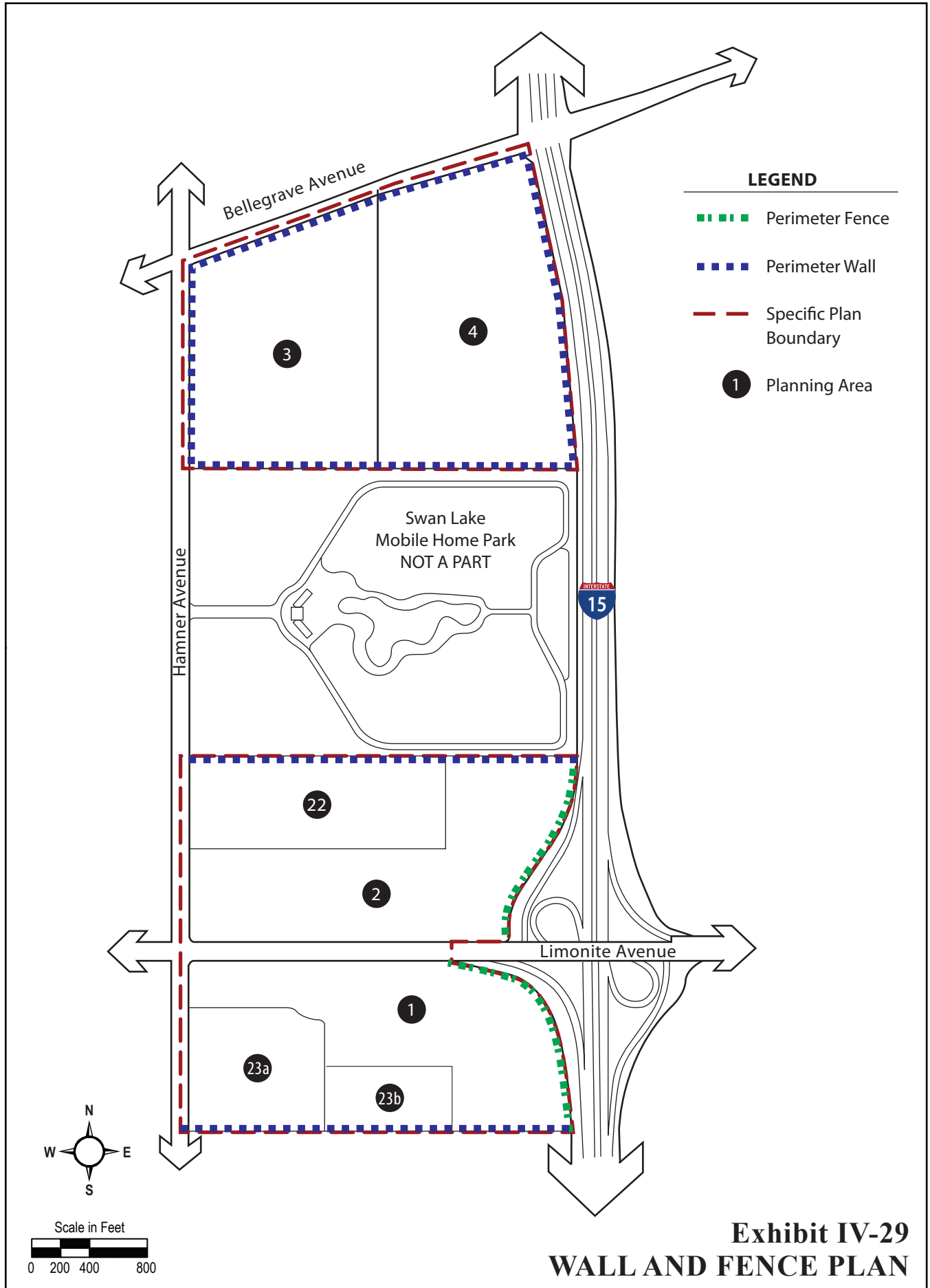
The *City of Eastvale I-15 Corridor Specific Plan* Wall and Fencing Plan is illustrated in **Exhibit IV-29, Wall and Fence Plan** and wall and fencing details are shown on **Exhibit IV-30, Perimeter Wall Elevations** and **Exhibit IV-31, Theme Wall Elevations**.

Two material types have been chosen as design elements to provide this continuity: (1) Taupe-colored, split-face block; and (2) Pre-cast concrete. Of these materials, split-face block is the primary material from which solid wall panels, and wall/fence pilasters are constructed. Concrete is the basis for pre-cast wall/pilaster caps and split-rails for fences. The use of these materials, in various combinations, creates the specific designs for each of the above-mentioned categories.

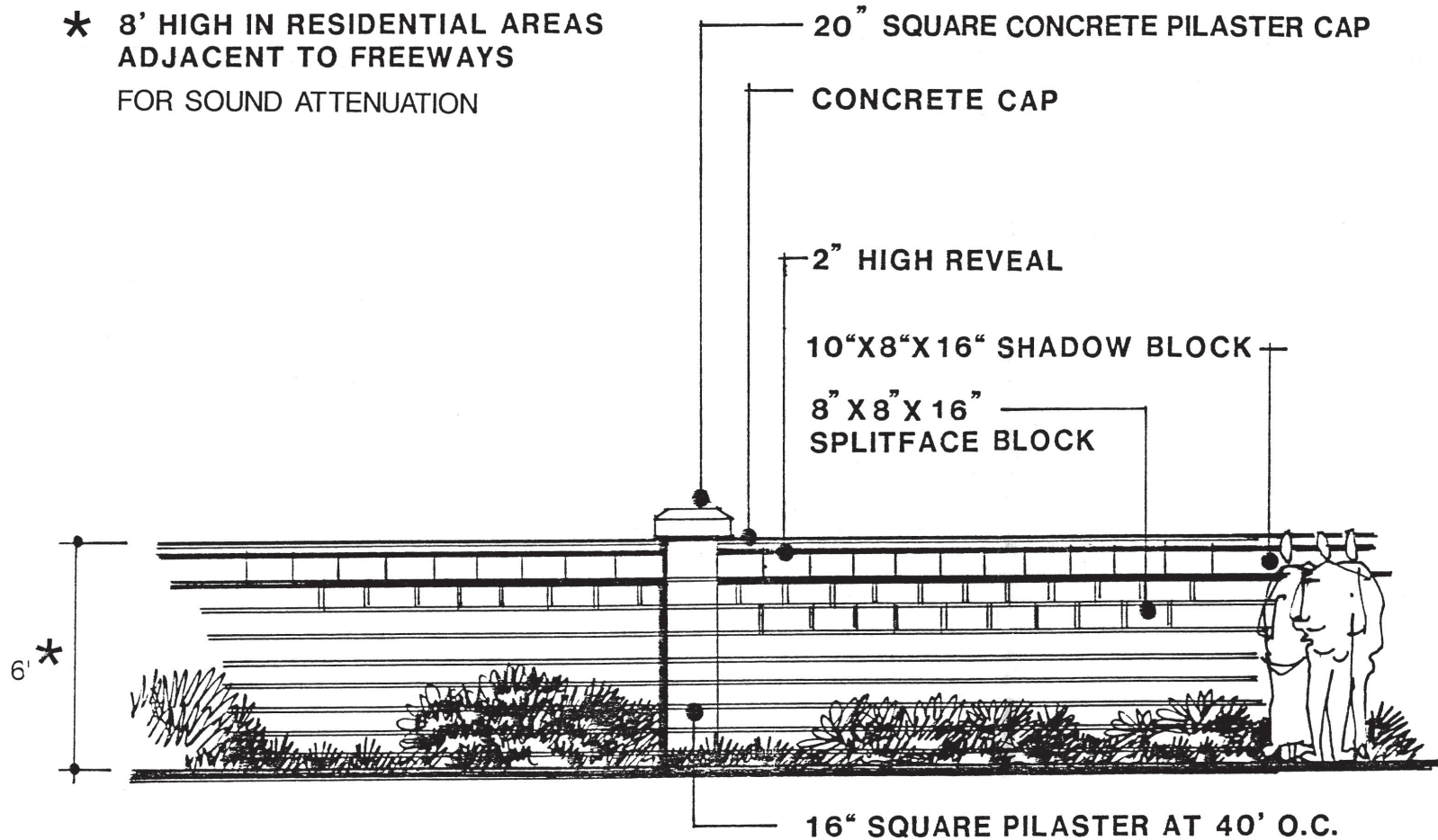
a. Wall and Fence Standards

- 1) Fence and wall heights shall generally be as tall as those objects or areas they are intended to visually screen, however, no fence or wall shall exceed three feet in height within any required front setback area, or exceed six feet in height within any required side or rear setback area. Sound attenuation barriers may be exempted from these standards upon City approval.
- 2) Where a commercial use abuts property in any residential zone, a masonry wall six feet in height and screen landscaping five feet in width shall be erected and maintained between such uses and the residential zone (see **Exhibit IV-27, Buffer Treatment Section** and **Exhibit IV-28, Buffer Treatment Plan**).
- 3) Fences and walls shall be utilized to visually screen and/or physically enclose outdoor storage areas, loading docks and ramps, transformers, storage tanks, and other appurtenant items of poor visual quality.
- 4) Fences and walls shall be used on the perimeter of properties to define property limits, separate use areas, and provide on-site security. Fencing, walls, and other

structural barriers shall be designed of similar materials, colors and general style as the primary buildings on a site.



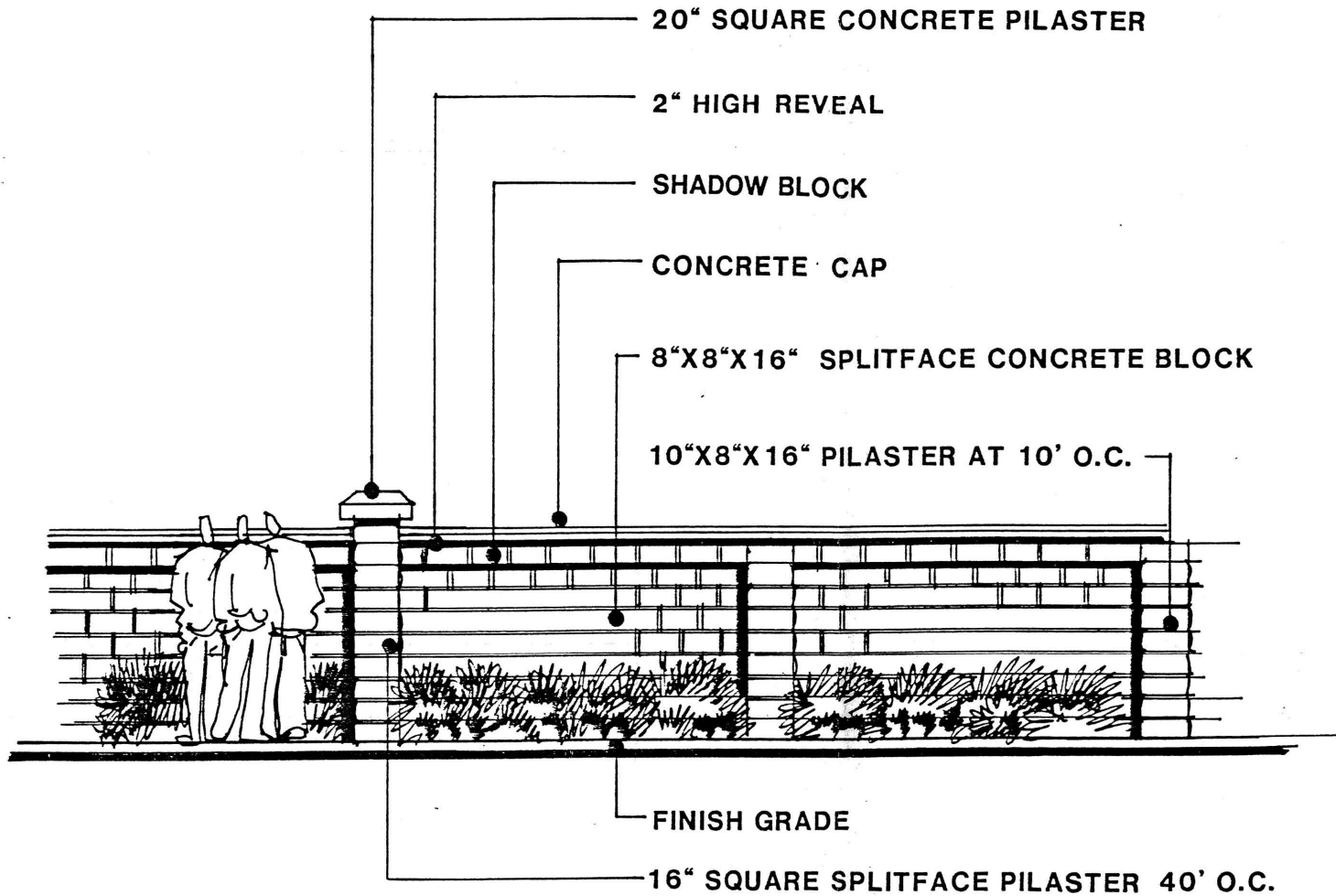
* 8' HIGH IN RESIDENTIAL AREAS
ADJACENT TO FREEWAYS
FOR SOUND ATTENUATION



PLANNING AND DESIGN SOLUTIONS
COMMUNITY PLANNING • URBAN DESIGN
• ENVIRONMENTAL SERVICES

Denné/Hourian Collaborative
LANDSCAPE ARCHITECTURE • PLANNING • URBAN DESIGN

Exhibit IV-30
PERIMETER WALL ELEVATION



PLANNING AND DESIGN SOLUTIONS
COMMUNITY PLANNING • URBAN DESIGN
• ENVIRONMENTAL SERVICES

Denné/Hourian Collaborative
LANDSCAPE ARCHITECTURE • PLANNING • URBAN DESIGN

Exhibit IV-31
THEME WALL ELEVATION

D. IMPLEMENTATION PROGRAMS

Although the *City of Eastvale I-15 Corridor Specific Plan* will be implemented through a variety of approval steps (i.e., zoning, site plans, parcel and tentative tract maps), the primary implementation tool is the Specific Plan itself which establishes the character of development through the definition of plan features, development standards and design concepts.

These plan features, development standards and design concepts have been described in Section IV. Together, they provide the framework for formulating Design Guidelines and Development Standards to be used at the site specific planning level to ensure that the intent of the Specific Plan is realized. The design concepts, plan features, Design Guidelines and Development Standards establish a basis upon which all subsequent implementing planning decisions can be based, and criteria for determining consistency of site specific design within the Specific Plan.

1. Zoning/General Plan

Rezoning of the property is necessary to bring the zoning designation into conformance with the Land Use Development Plan adopted as a part of the Specific Plan. A Change of Zone application has been filed and is being processed concurrently with this Specific Plan. Planning areas 23a and 23b are currently zoned Specific (SP) so no change is necessary. The change of zone applies to Planning Areas 1, 2 and 22 to change the zoning from Scenic Highway Commercial (C-P-S) to Specific Plan (SP).

A General Plan Amendment is necessary to bring the land use designations of undeveloped planning areas into conformance with the current General Plan. A General Plan Amendment has been filed and is being processed concurrently with this Specific Plan. The General Plan Amendment applies to Planning Area 23b and changes the General Plan land use designation of the southern 10 acres of Planning Area 23b from High Density Residential (HDR) designations to Highest Density Residential (HHDR) and the northern 5 acres of Planning Area 23b from HDR to Commercial-Retail (CR).

2. Review Procedures

The *City of Eastvale I-15 Corridor Specific Plan* shall be implemented through the Parcel Map, the Tentative Tract Map and Site Plan Review process.

Subsequent to the approval of the Specific Plan and prior to, or concurrent with, the initial Tentative Tract Map, Site Plan Review, a Parcel Map may be submitted. The Parcel Map submittal will meet all requirements stipulated by the Subdivision Map Act. All lots created by parcel maps shall be consistent with the area specifications for the planning areas within the

Specific Plan. An adjustment in acreage of up to 10 percent constituting a refinement of the Specific Plan shall be allowed.

Tentative Tract Maps shall be required for implementing detached single-family residential projects, or multi-family residential projects as required by the Subdivision Map Act. After Tentative Tract Map approval, the Final Tract Map may be recorded and building permits may be issued. This process may include the parcelization of a lot or lots for future use as a planned unit development, condominium, apartment, or cluster development site.

All subdivision within the *City of Eastvale 1-15 Corridor Specific Plan* shall be consistent with the provisions, procedures and requirements of City of Eastvale Ordinance. Plot Plan approval shall be required for all implementing condominium or apartment developments, and commercial projects. Plot Plan approval is described below, and may occur either concurrent with or subsequent to parcelization of the development site.

Conditional Use Permits are required for certain uses pursuant to the City Zoning Code. Procedure for processing of Conditional Use Permits is described below.

a. Final Site Development Plan/Plot Plan

All implementing projects which require a Plot Plan or final Site Development Plan pursuant to the City Zoning Code shall be processed in accordance with the provisions of the City Zoning Code.

b. Conditional Use Permits

Conditional Use Permits shall be processed in accordance with the provisions of Section 18.28, the City Zoning Code.

3. Transfer of Dwelling Units/Density

Minor variations to planning areas may occur through implementation of the Specific Plan. The following provisions for the transfer of dwelling units will serve both the public and the planned community, it will:

- Minimize City staff, Council and Commission time spent on minor adjustments and technical matters.
- Set criteria/findings that demonstrate consistency with Specific Plan goals and objectives.
- Assures that levels of development throughout the life of the Specific Plan are consistent with the adequacy of available or proposed infrastructure and public services.

Planning area boundaries may be modified with the approval of the City of Eastvale Planning Department. The numbers of units within each planning area may be increased to a maximum

of 10 percent, provided that the number of units for the entire Specific Plan area is not increased. The density transfer is further contingent upon the existence of excess units from a previously approved tentative map for a planning area not containing the maximum number of units delineated by the Land Use Plan.

Transfer of dwelling units between planning areas that do not exceed the total units approved (1,596) but result in changes to the maximum density of a planning area shall be subject to approval of the Planning Director. The transfer shall be approved based on the following findings:

- a. That the total dwelling units and development area as approved in this Specific Plan is not exceeded;
- b. That demands on parks, schools and community facilities are not significantly affected;
- c. That demands on public improvements and infrastructure are not exceeded;
- d. That grading and landform alteration does not significantly differ from that approved by the Specific Plan; and
- e. That the overall design and visual quality of the planned community is not significantly affected.

4. Specific Plan Amendments

Amendments to this Specific Plan shall be processed in accordance with the provisions of Title 7, Division 1, Chapter 3, Article 9 of the California Government Code.

The following changes to this Specific Plan may be made without amending the plan:

- The transfer of dwelling units from one planning area to another, provided the proper accounting is included in the plan as outlined in this section.
- The addition of new information to this Specific Plan maps or text that does not change the effect of any regulation.
- Changes to the community infrastructure such as drainage systems, roads, water and sewer systems, etc., which do not have the effect of increasing capacity beyond the maximum amount of dwelling units allowed in this Specific Plan.

The following changes to this Specific Plan will require a Specific Plan Amendment:

- Changes to the text or maps of this Specific Plan other than the addition of new information which does not change the effect of any regulation.
- An increase (not transfer) in dwelling unit allocation beyond that exceeds the maximum amount of dwelling units allowed in this Specific Plan.
- Major changes in infrastructure such as drainage systems, roads, water and sewer systems, etc., which have the effect of increasing capacity beyond the maximum amount of dwelling units allowed in this Specific Plan.

Changes to this Specific Plan map or text described above which do not require a Specific Plan Amendment may be made in an administrative manner without ordinance amendment. The following development parameters shall be considered as part of any amendment to the Specific Plan Land Use Development Plan:

- Basic Infrastructure System Design: Capacity and location of major roads and water, sewerage and drainage facilities; and
- Surrounding Land Uses: Existing uses within and adjacent to study area.

5. Development Monitoring Program

The purpose of the Development Monitoring Program is to set forth a system whereby periodic adjustments in density and dwelling unit types within Specific Plan planning areas may be accomplished. Market changes may dictate revisions such as these which result in minor modifications to the overall plan and without exceeding the overall dwelling unit yield of the plan.

The Development Monitoring Program effectively establishes an accounting system to ensure that all changes, upon approval, are properly recorded at the scale of the total project and each planning area reflected in this Specific Plan Land Use Development Plan.

In order to accommodate possible changes and to ensure conformance with City of Eastvale Ordinance, the following provisions shall guide and govern incremental allocation and provision of residential dwelling units within the Specific Plan study area.

- The overall assigned dwelling unit yield of 1,596 residential dwelling units shall not be exceeded.
- A site-specific planning package shall be submitted to the City of Eastvale for review and approval prior to development occurring in any planning area. The package shall consist of Plot Plans pursuant to the City Zoning Code, and/or Tract or Parcel Maps as required by City Ordinance. Such plans shall be subject to the conditions of approval set forth by the City of Eastvale.
- Density variations or changes in numbers of dwelling units that constitute significant deviations from this Specific Plan shall be approved by the City Planning Department, or, upon their referral, by Planning Commission action.
- Any approved unit total refinement or deviation shall be accompanied by a revised statistical table in all text and map locations where unit counts are reflected. Said table shall show new dwelling unit totals for each planning area in which a change is made. All drafts of such tables and the final approved version shall be identified by a revision date located under the title block.

6. Development Agreement

Pursuant to state law¹ a development agreement may be drafted that will specify the permitted land uses, land use intensities and development standards of this Specific Plan. Upon approval of this Specific Plan, the development agreement can be prepared based on the approved Specific Plan which has been prepared consistent with the Eastvale Area Plan and the General Plan. Pursuant to Section 65867.5 of Article 2.5, the development agreement must be consistent with the General Plan as well as any applicable Specific Plan.

The law² states that the agreement shall consist of the items listed above, and may include conditions, terms, restrictions and requirements for subsequent discretionary actions. These conditions, terms, restrictions and requirements shall not prevent development of the land for the uses, and/or alter the intensity of development set forth in the agreement. The agreement may provide that construction shall begin within a specified time and that implementing project or any phase thereof be completed within a specified timeframe.

7. Property Owner Notification

Covenants, Conditions and Restrictions (CCRs) for implementing projects will include notification, prior to sale, to all Specific Plan property owners adjacent to existing and future residential areas that are zoned for the allowance of equestrian lots. The notification shall state that the adjacent property is zoned as such and permits the keeping of horses and other animals.

¹ Article 2.5, Chapter 4, Zoning Regulations of the State Planning, Zoning and Development Laws, 1989.

² Article 2.5, Chapter 4, Zoning Regulations of the State Planning, Zoning and Development Laws, 1989, Section 65865.2.

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